

**MEMORANDUM IN SUPPORT OF APPLICATION FOR  
SPECIAL PERMIT**

**PROPOSED DEVELOPMENT:**

**UNION SQUARE VILLAGE IN WALPOLE, MA  
DATED: JANUARY 30, 2024**

**DEVELOPER/APPLICANT**

**WALL STREET DEVELOPMENT CORP.  
P.O. BOX 272, WESTWOOD, MA 02090  
TEL. (617) 922-8700**

## SECTION I - DEVELOPMENT OVERVIEW

### 1. Development Summary:

The proposed development, entitled Union Square Village (the "Development"), calls for the construction of Fifteen (15) detached, single family condominium homes on 3.60+/- acres, more or less. Each of the units will be designed with an attached two-car garage, 2-3 bedrooms, 2 1/2 baths, kitchen, dining room/dinette, family room and study. It is anticipated each of the homes will be approximately 1,900 - 2,400 square feet of living area. In addition, private outdoor spaces, i.e. patios, decks, etc. will be provided depending on site constraints. The proposed homes will be serviced by municipal water and sewer, as well as, natural gas, telephone, cable, etc. all of which will be installed underground. As each of the units will be single family in nature, the proposed homes will blend quite nicely with the adjacent neighborhood, which consists of a combination of single family and two-family homes. The proposed Development and all site improvements are shown on a plan entitled, "Special Permit & Site Development Plan – Union Square Village" dated January 24, 2024, prepared by GLM Engineering Consultants, Inc., 19 Exchange Street, Holliston, MA 01746 (the "Site Plan"). A copy of the Site Plan is included with this application as Exhibit I.

### 2. Zoning District:

The entire property and proposed development is located within the General Residence (GR) zoning district in the Town of Walpole.

### 3. Location:

The development site consists of Four (4) parcels of land. Parcel I is a vacant parcel of land containing approximately 53,143 +/- sq. ft., with roadway access from Union Street. Parcel II is a vacant parcel of land containing approximately 54,679 +/- sq. ft., with access available from Burns Avenue. Parcel III is a parcel of land containing approximately 34,402 +/- sq. ft (formerly known as 48 Burns Avenue). Parcel IV is known as 7 Brook Lane, consisting of an existing single-family home on 16,537 +/- sq. ft. The total land area contained in the development site is 157,373 sq. ft. or 3.61 +/- acres.

### 4. Present Use(s):

The development site is predominantly vacant land with the exception of a single-family home at 7 Brook Lane. This home is proposed to be removed from the site.

### 5. Applicant:

Wall Street Development Corp.  
P.O. Box 272  
Westwood, MA  
Tel. (617) 922-8700  
Email. lou@wallstreetdevelopment.com

6. Property Owner Information:

Wall Street Development Corp.  
P.O. Box 272  
Westwood, MA 02090  
Tel. (617) 922-8700  
Email. lou@wallstreetdevelopment.com

Assessors Map: 20, Parcel: 119, 136 &137  
Book: 34158 Page: 536

Brian Martin  
7 Brook Lane  
Walpole, MA 02032  
Tel. (508) 942-9189  
Email.

Assessors Map: 20, Parcel: 115  
Book: 25238 Page: 585

## SECTION II - REQUESTED ACTIONS

### 1. Special Permit Application:

The application before the Walpole Board of Appeals requests a Special Permit under Section 6-C.4.A of the Walpole Zoning Bylaws to allow "more than one principal building on a lot where the applicant has also shown that the design and the placement of the buildings will be in harmony with the general character of the neighborhood streetscape, driveways, building placement, setbacks, volume and spacing" in a General Residence District. All of which has been shown on the Site Plan included as a part of this application.

### 2. Special Permit Review Criteria under Section 2 of the Walpole Zoning Bylaw:

Special Permits may be granted by the Board of Appeals only for uses which are in harmony with the purposes and intent of this Bylaw and which shall be subject to any general or specific rules prescribed herein and to any appropriate conditions, safeguards and limitations on time and use.

Prior to granting a Special Permit, the Board of Appeals shall make a finding and determination that the proposed use, building, structure, sign, parking facility or other activity which is the subject of the application for the Special Permit:

- a) does and shall comply with such criteria or standards as shall be set forth in the section of this Bylaw which refers to the granting of the requested Special Permit;
- b) shall not have vehicular and pedestrian traffic of a type and quantity so as to adversely affect the immediate neighborhood;
- c) shall not have a number of residents, employees, customers or visitors, so as to adversely affect the immediate neighborhood;
- d) shall comply with the dimensional requirements applicable to zoning district in which the premises is located, including, without limitation, the applicable lot coverage and buffer zone requirements in Section 5-G;
- e) shall not be dangerous to the immediate neighborhood of the premises through fire, explosion, emission of wastes, or other causes;
- f) shall not create such noise, vibration, dust, heat, smoke, fumes, odor, glare or other nuisance or serious hazard so as to adversely affect the immediate neighborhood;
- g) shall not adversely affect the character of the immediate neighborhood;
- h) shall not be incompatible with the purpose of the zoning Bylaw or the purpose of the zoning district in which the premises is located.

3. Review Criteria Facts:

The proposed Development is in compliance or meets the criteria outlined in Section 2 of the Walpole Zoning Bylaws, and is in harmony with the stated purposes of the Bylaw, as follows:

- 1) Included as part of this Special Permit application as Exhibit II is a "Traffic Impact and Access Study" dated February 1, 2023, prepared by Kimley-Horn of Waltham, MA. This report, which was previously prepared in connection with the development of 20-dwellings on the Property, concludes and has determined that "the analyses demonstrate that the roadways and intersections within the study area are able to accommodate the additional traffic with the proposed development project. Site traffic is expected to be able to enter and exit the site safely and efficiently. Safe site distance with respect to the site drive locations will be provided with the planned site drive construction. The additional traffic volumes associated with the proposed development site will not adversely affect the immediate neighborhood."
- 2) As previously summarized, the Development will consist of Fifteen (15) detached, single family condominium homes on 3.61 +/- acres. Each of the homes will be designed with an attached two-car garage, 2-3 bedrooms, 2 1/2 baths, kitchen, dining room/dinette, family room and study. Due to the size and number of the proposed condominiums, the Development will not have any significant increase in the number of residents that would adversely affect the immediate neighborhood;
- 3) This memorandum includes Table I entitled "Zoning Requirement/Comparison Table". As noted in the Table I, the proposed Development meets or otherwise complies with the dimensional requirements applicable to the General Residence zoning district, including buffer zone requirements;
- 4) As a residential development there will be no storage of any above ground or underground fuel tanks or other material that would be considered hazardous in any way. Based on these factors, the proposed Development will not be dangerous to the immediate neighborhood through fire, explosion, emissions of waste or other causes;
- 5) The completed construction of the Development will not result in any noise, vibration, dust, heat, smoke, fumes, odor, glare or other nuisance or serious hazard that would adversely affect the immediate neighborhood. Any proposed street lighting to be provided will be designed to not shine onto any abutting properties;
- 6) The proposed development has been designed as a neighborhood cluster of Fifteen (15) detached, single family condominium homes. Each of the homes will be reflective of traditional New England character. In addition, as shown on the Site Development Plan, the proposed homes will be consistent with the design and character of the surrounding area, such as but not limited to height, mass, setbacks, and traditional building materials have been incorporated.

In addition, the development site is in a neighborhood area of mixed single family and multi-family residences. The proposed detached single family condominium homes will be consistent with the existing homes in the neighborhood and will be similar in size and scope as the existing homes and therefore not adversely affect the character of the immediate neighborhood. Included as Exhibit III is a sampling of the exterior elevations proposed for the homes;

- 7) The Development site is located in a General Residence zoning district and the proposed construction of detached, single family condominium homes will be compatible with the immediate neighborhood, specifically, and the General Residence zoning district, generally. It can be safely concluded the proposed Development is compatible with the purpose of the Walpole Zoning Bylaw.
- 8) The proposed Development site has been evaluated and designed to provide proper storm-water management in accordance with state and local regulations. A Stormwater Management Report prepared by GLM Engineering Consultants, Inc. of Holliston, MA will be provided by separate cover.

Based on the above information, it is requested the Walpole Board of Appeals grant a Special Permit under Section 6-C.4.A of the Walpole Zoning Bylaws to allow the construction and development of Fifteen (15) detached, single-family, condominium homes and associated site utilities, access drives, landscaping, etc. al as shown on the plan entitled ""Special Permit & Site Development Plan – Union Square Village” dated January 24, 2024, prepared by GLM Engineering Consultants, Inc., 19 Exchange Street, Holliston, MA 01746, as may be amended during the Special Permit granting process.

Respectfully Submitted,

**WALL STREET DEVELOPMENT CORP.**

  
BY: Louis Petrozzi, President      1/30/2024  
Date

**TABLE I  
UNION SQUARE VILLAGE  
ZONING REQUIREMENTS/COMPARISON TABLE  
GENERAL RESIDENCE ZONING DISTRICT**

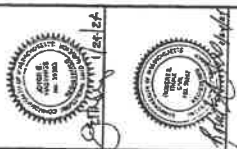
<u>Description</u>	<u>Required</u>	<u>Proposed</u>
Lot Area:	150,000 S.F.	157,373 S.F.
Lot Frontage:	100 FT.	110 FT. (Brook Lane)
Yard Setbacks:		
Front:	30 FT.	31.0 FT.
Side:	10 FT.	22.0 FT.
Rear:	30 FT.	30.0 FT.
Lot Coverage - Structures:	30% 47,212 S.F.	15.5 % 24,344 S.F.
Lot Coverage - Impervious:	50% 78,686 S.F.	34.6% 54,493 S.F.
Maximum Building Height:	35 FT.	35FT.
Maximum Stories:	2.5 Stories	2.5 Stories

# **EXHIBIT I**





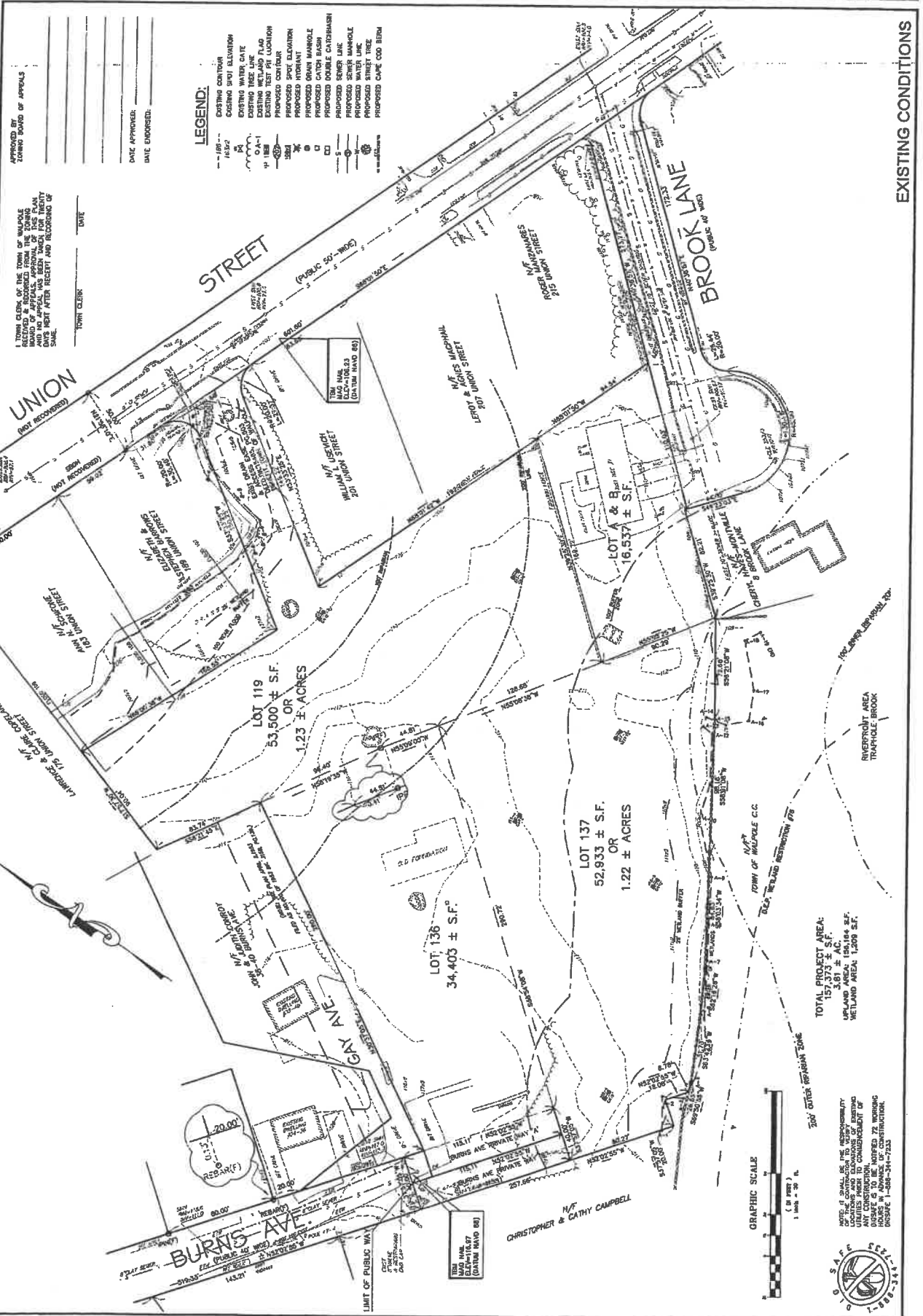
NO.	DATE	REVISIONS



**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**WALPOLE SQUARE VILLAGE**  
**WALPOLE, MASSACHUSETTS**  
 NETWORK  
 WALL STREET DESIGN/PLANNING CORP.  
 P.O. BOX 272  
 WESTWOOD, MA 02090

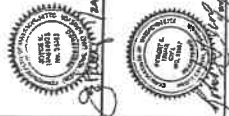
**WJLM Engineering, Inc.**  
 10 EXCHANGE STREET  
 WESTWOOD, MA 02090  
 P. 608-429-1100  
 F. 608-429-7180  
 www.wjlmengineering.com

JOB NO. 15688  
 DATE: JAN. 24, 2024  
 SCALE: 1" = 30'  
 SHEET: 2 of 13  
 PLAN#: 27,096



**EXISTING CONDITIONS**

NO.	DATE	DESCRIPTION



**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**UNION SQUARE VILLAGE**  
**WALPOLE, MASSACHUSETTS**  
 ARCHITECT:  
 WALL STREET DEVELOPMENT CORP.  
 P.O. BOX 272  
 WESTWOOD, MA 02090

GLM Engineering, Inc.  
 18 EXCHANGE STREET  
 WILMINGTON, MA 01748  
 TEL: 508-653-1100  
 FAX: 508-628-7180  
 www.GLMeng.com

JOB No. 15,588  
 DATE: JAN. 24, 2024  
 SCALE: 1" = 30'  
 SHEET: 3 of 12  
 PLAN #: 27,096

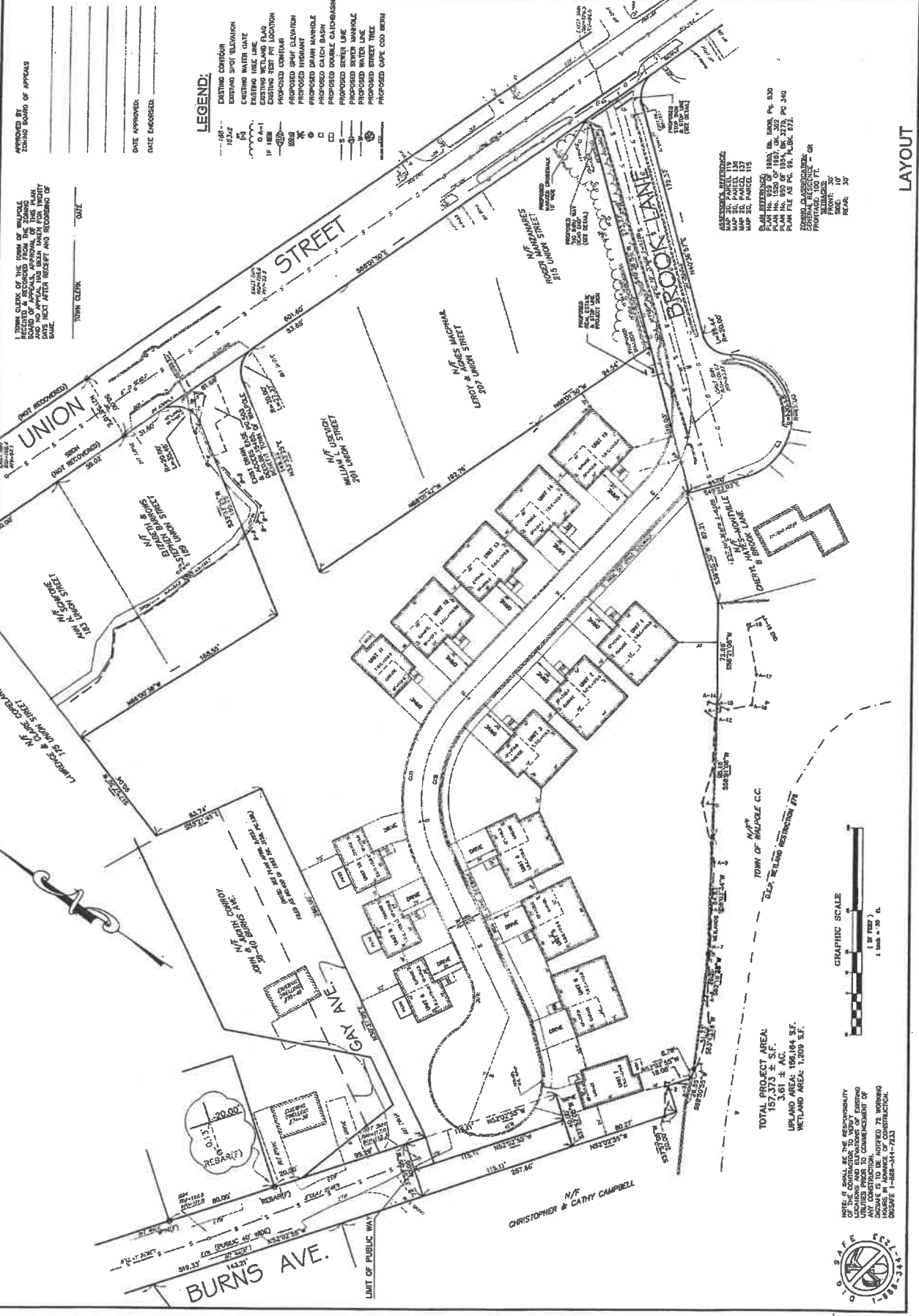
APPROVED BY:  
 TOWN BOARD OF APPEALS

TOWN CLERK \_\_\_\_\_ DATE \_\_\_\_\_

DATE APPROVED: \_\_\_\_\_  
 DATE ENDED: \_\_\_\_\_

- LEGEND:**
- EXISTING CONTOUR
  - EXISTING SPOT ELEVATION
  - EXISTING WATER GATE
  - EXISTING FIRE LINE
  - EXISTING WETLAND FLAG
  - EXISTING CONTROL LOCATION
  - PROPOSED SHUT SWITCHEM
  - PROPOSED DRAIN MANHOLE
  - PROPOSED DOUBLE CATCHBASIN
  - PROPOSED STREET LIGHT
  - PROPOSED WATER LINE
  - PROPOSED STREET TREE
  - PROPOSED GUYE ODD BERRY

I, TOWN CLERK OF THE TOWN OF WALPOLE, MASSACHUSETTS, HEREBY CERTIFY THAT THE BOARD OF APPEALS HAS REVIEWED AND APPROVED THIS PLAN FOR THE REASONS SET FORTH HEREIN AND THAT THE BOARD HAS NOT WITHHELD OBJECTION THEREOF.



ASSEMBLY REFERENCE:  
 MAP 20, PARCEL 138  
 MAP 20, PARCEL 139  
 MAP 20, PARCEL 140

PLAN REFERENCES:  
 PLAN No. 252 OF 1800, RL 5006, Pp. 830  
 PLAN No. 253 OF 1800, RL 5006, Pp. 830  
 PLAN No. 254 OF 1800, RL 5006, Pp. 830  
 PLAN No. 255 OF 1800, RL 5006, Pp. 830  
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 PLAN No. 270 OF 1800, RL 5006, Pp. 830



NOTE: IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE ACCURACY OF THE DATA AND DIMENSIONS SHOWN HEREON PRIOR TO COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS IN THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS IN THIS PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS IN THIS PLAN.



LAYOUT



NO.	DATE	DESCRIPTION



**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**UNION SQUARE VILLAGE**  
**WALPOLE, MASSACHUSETTS**

APPLICANT:  
 WALL STREET DEVELOPMENT CORP.  
 P.O. BOX 272  
 WESTWOOD, MA 02090

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 F: 508-428-7160

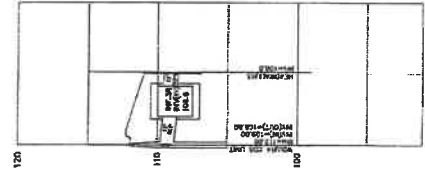
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 SHEET: 5 of 12  
 PLAN #: 27.098

APPROVED BY:  
 ZONING BOARD OF APPEALS

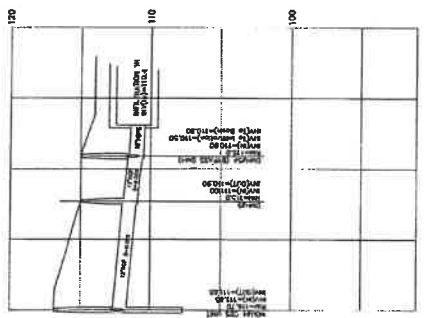
DATE APPROVED: \_\_\_\_\_  
 DATE ENDED: \_\_\_\_\_

TOWN CLERK: \_\_\_\_\_ DATE: \_\_\_\_\_

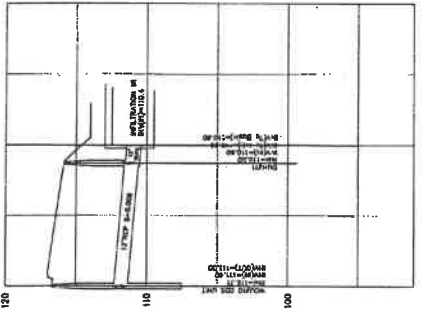
I, TOWN CLERK OF THE TOWN OF WALPOLE, MASSACHUSETTS, HEREBY CERTIFY THAT THE BOARD OF APPEALS, APPLICANT OF THIS PLAN, HAS COMPLIED WITH ALL REQUIREMENTS AND THAT THIS PLAN IS IN ACCORDANCE WITH ALL TOWN ORDINANCES AND REGULATIONS AND IS SUBJECT TO THE SAME.



**PROFILE DRAIN LINE**  
**DMH#14 TO HEADWALL#15**  
 SCALE: Horiz 1"=40', Vert 1"=4'



**PROFILE DRAIN LINE**  
**DMH#14 TO HEADWALL#6**  
 SCALE: Horiz 1"=40', Vert 1"=4'



**PROFILE DRAIN LINE**  
**DMH#10 TO HEADWALL#11A**  
 SCALE: Horiz 1"=40', Vert 1"=4'

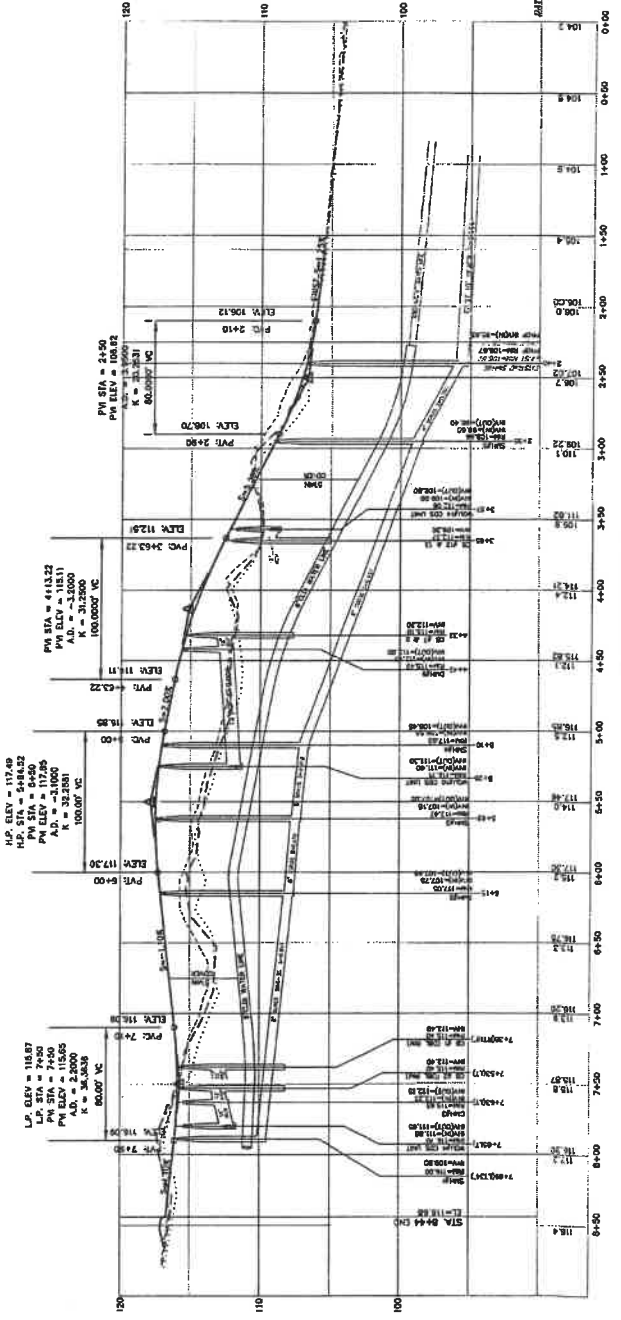
**Drainage System Table:**

Structure	Rim (In.)	Invert (In.)	Invert (Out.)
CS#1	115.40	112.25	112.40
CS#2	115.40	112.25	112.40
DMH#3	115.05	111.85	111.65
DMH#4	115.70	111.85	111.65
DMH#5	115.00	110.90	110.90
DMH#5A	115.00	110.90	110.90
CS#7	115.10	112.20	112.20
CS#8	115.10	112.10	112.20
CS#9	115.10	112.10	112.20
WOU#10	115.70	112.40	112.30
DMH#11	115.90	110.80	110.50
CS#12	115.37	108.20	108.20
CS#11	114.80	108.60	108.60
WOU#14	114.06	109.00	108.80
WU#15	114.06	109.00	108.50

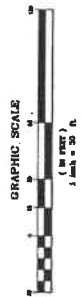
  

**Sewer System Table:**

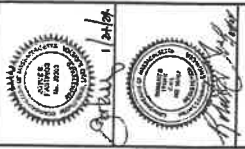
Structure	Rim (In.)	Invert (In.)	Invert (Out.)
SMH#1	112.05	105.80	105.50
SMH#2	112.05	105.80	105.50
SMH#3	112.05	105.87	105.87
SMH#4	112.03	105.35	105.25
SMH#5	105.96	98.90	98.40
ENST#6	105.97	95.85	95.17



**PROFILE DRIVE 'A'**  
 SCALE: Horiz 1"=40', Vert 1"=4'



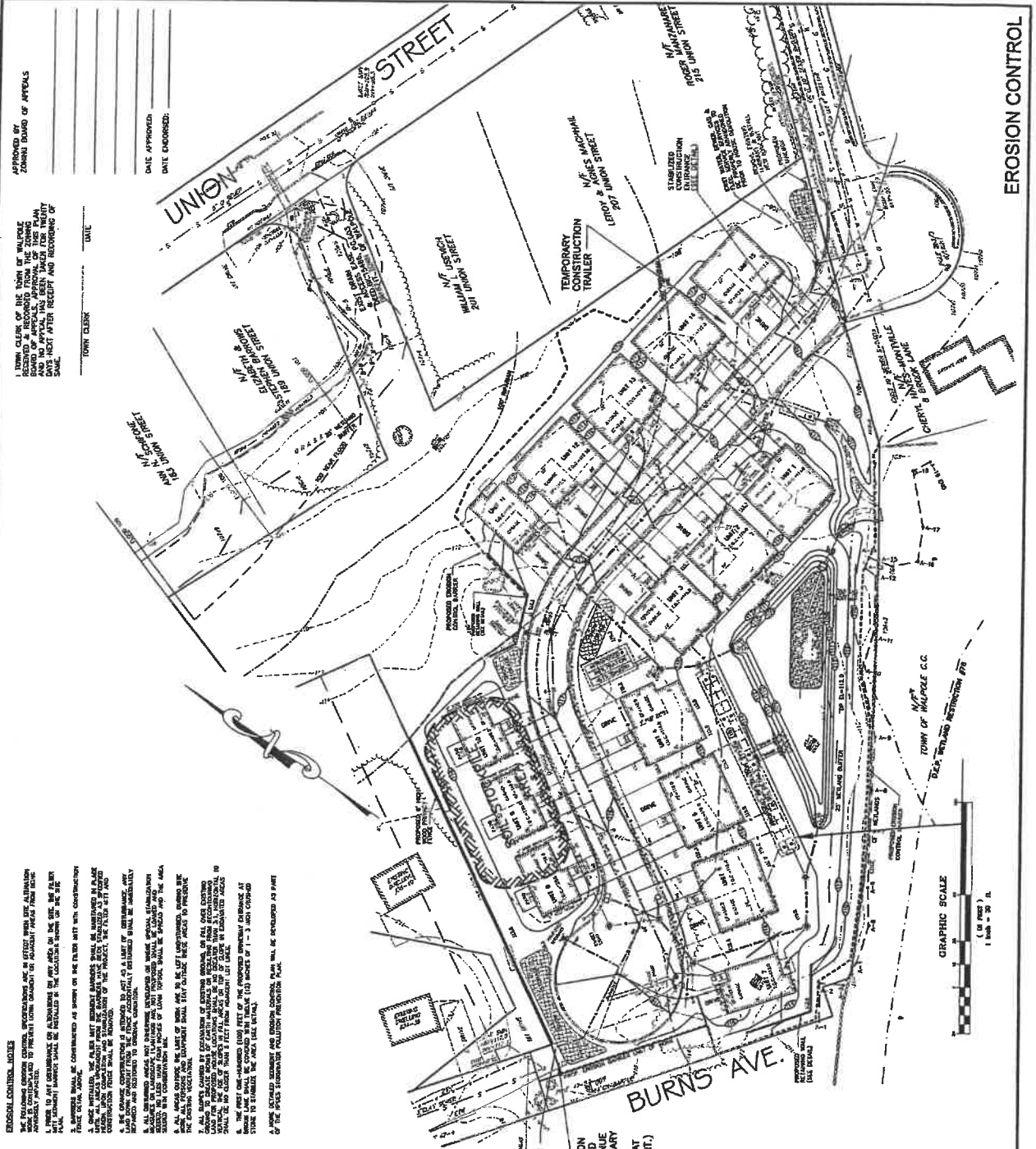
NO.	DATE	DESCRIPTION



**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**UNION SQUARE VILLAGE**  
**WALPOLE, MASSACHUSETTS**  
 DEVELOPER:  
 WALL STREET DEVELOPMENT CORP.  
 P.O. BOX 272  
 WESTWOOD, MA 02090

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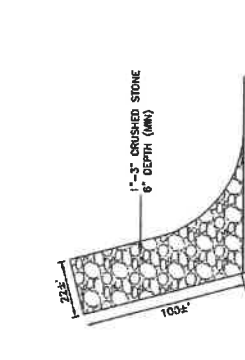
JOB No. 15,588  
 DATE: JAN 24, 2004  
 SCALE: 1" = 30'  
 SHEET: 6 of 12  
 PLANS: 27,096



**EROSION CONTROL MAINTENANCE:**  
 THE CONTRACTOR SHALL HAVE ON THE SITE AN ADEQUATE QUANTITY OF STONE AND SACKS TO BE USED FOR THE CONTROL OF EROSION AND SILTATION. MEASURES SHALL BE INSPECTED WEEKLY AND AFTER A STORM EVENT OF 1" OR GREATER.

**GENERAL:**  
 STORM AND DRAINAGE SHALL BE DONE IN A MANNER WHICH WILL NOT CAUSE DAMAGE TO ADJACENT PROPERTIES. FROM A DRAINAGE CONSTRUCTION PROCEDURE, ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. THE SLOTTED BASIN SHALL BE A SLOTTED BASIN AS SHOWN ON THE DRAWING. THE SLOTTED BASIN SHALL BE LOCATED AT LEAST 100 FEET UPSTREAM FROM ANY NEARBY RESIDENCE AREA.

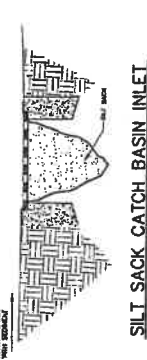
**STORMWATER:**  
 STORMWATER SHALL BE STORED IN THE AREAS DESIGNATED ALL NEARBY AREAS SHALL BE SECURED AROUND THE PERIMETER WITH BARRIERS ONE OR TWO FEET HIGH.



**STABILIZED CONSTRUCTION ENTRANCE**  
 NOT TO SCALE



**FILTER MITT WITH ORANGE CONSTRUCTION FENCE DETAIL**  
 NOT TO SCALE



**SILT SACK CATCH BASIN INLET**  
 NOT TO SCALE

**EROSION CONTROL NOTES:**  
 1. THE FOLLOWING EROSION CONTROL MEASURES ARE TO BE INSTALLED IMMEDIATELY UPON THE START OF CONSTRUCTION AND MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.  
 2. EROSION CONTROL MEASURES SHALL BE INSTALLED IN THE COURSE OF THE CONSTRUCTION PERIOD.  
 3. EROSION CONTROL MEASURES SHALL BE MAINTAINED AS NECESSARY THROUGHOUT THE CONSTRUCTION PERIOD.  
 4. EROSION CONTROL MEASURES SHALL BE MAINTAINED AS NECESSARY THROUGHOUT THE CONSTRUCTION PERIOD.  
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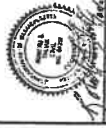
**EROSION CONTROL NOTES:**  
 19. EROSION CONTROL MEASURES SHALL BE MAINTAINED AS NECESSARY THROUGHOUT THE CONSTRUCTION PERIOD.  
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 24. EROSION CONTROL MEASURES SHALL BE MAINTAINED AS NECESSARY THROUGHOUT THE CONSTRUCTION PERIOD.

**EROSION CONTROL**



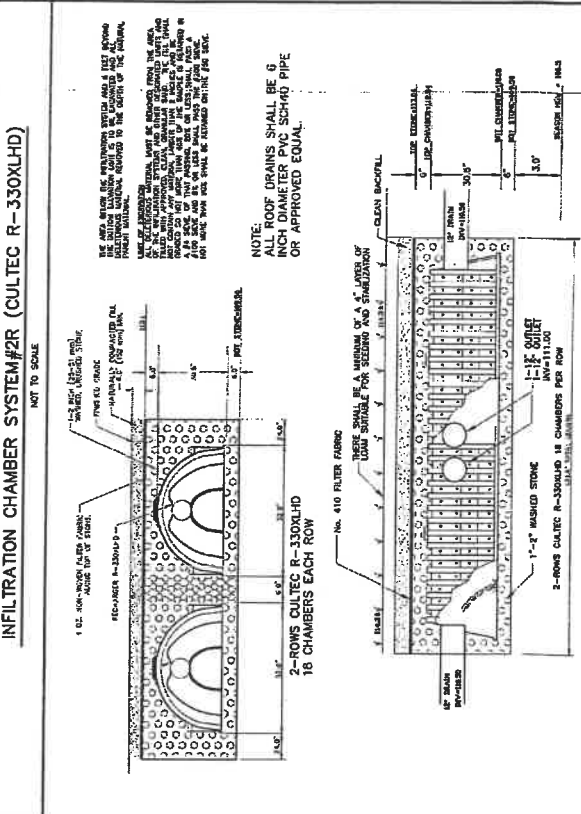
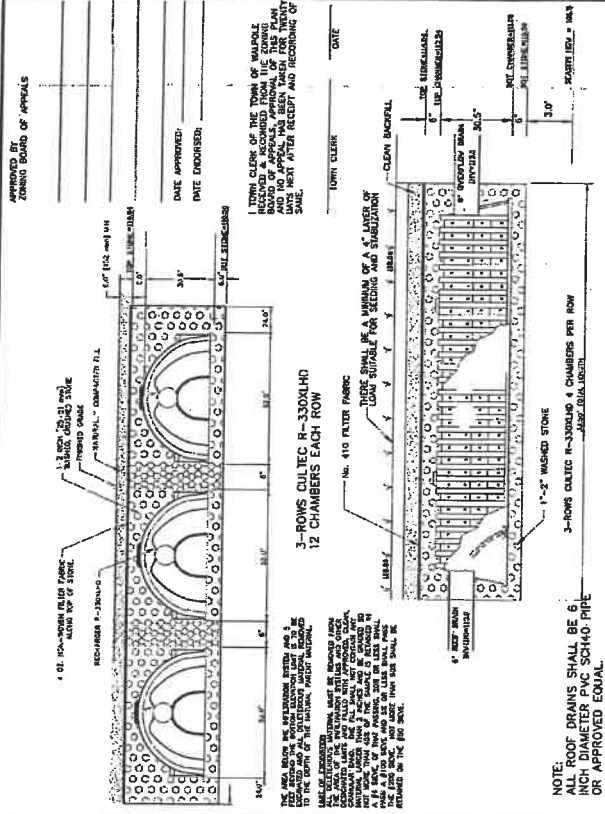
No.	DATE	REVISIONS	DESCRIPTION



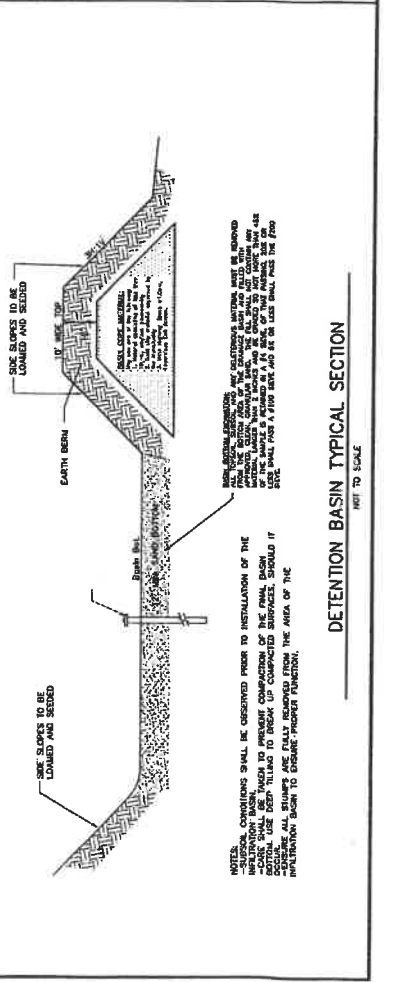
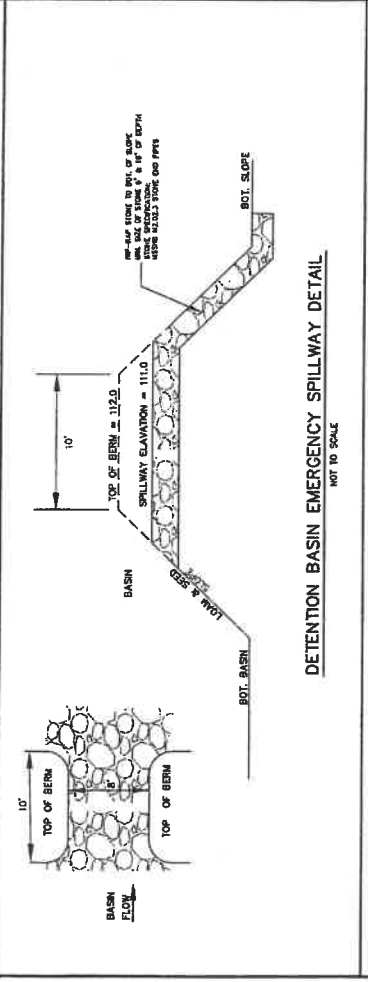
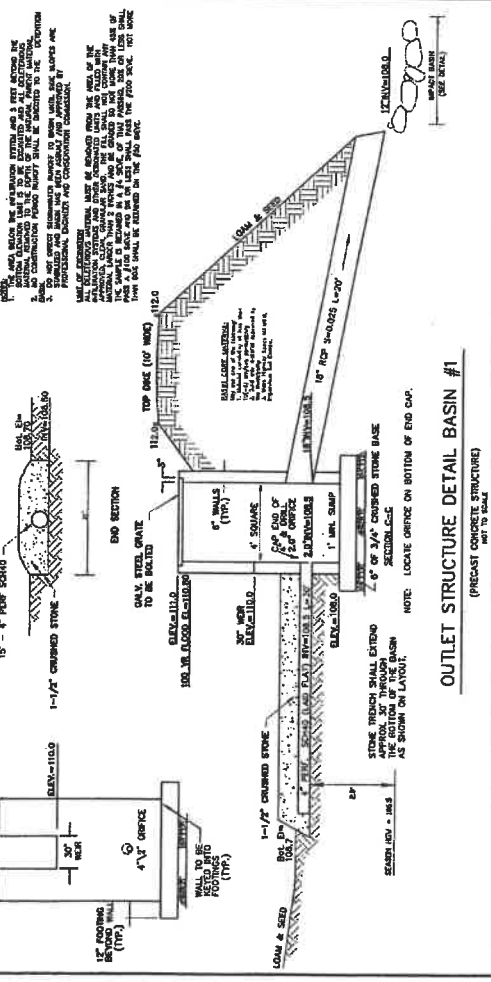
**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**UNION SQUARE VILLAGE**  
**WALPOLE, MASSACHUSETTS**  
 WALL STREET DEVELOPMENT CORP.  
 P.O. BOX 272  
 WESTWOOD, MA 02090

**CLM Engineering, Inc.**  
 19 EXCHANGE STREET  
 WILMINGTON, MA 01897  
 P: 508-438-1100  
 F: 508-438-7160  
 www.clmengineering.com

JOB No. 15,888  
 DATE: JAN 28, 2004  
 SCALE: 1" = 30'  
 SHEET: 7 OF 12  
 PLAN #: 27,096



**DETAILS**



NOTE:  
 1. VERIFY THE PROPOSED SPILLWAY AND 5 FEET ABOVE THE SPILLWAY TO BE CONSTRUCTED TO THE TOP OF THE SPILLWAY. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY.

2. DO NOT EXCEED THE MAXIMUM ALLOWED SPILLWAY HEIGHT AS SHOWN ON THE SPILLWAY DETAIL. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY.

3. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY.

4. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY.

5. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY. THE SPILLWAY SHALL BE CONSTRUCTED TO THE TOP OF THE SPILLWAY.

NOTE:  
 ALL ROOF DRAINS SHALL BE 6 INCH DIAMETER PVC SCH40 PIPE OR APPROVED EQUAL.

NOTE:  
 SLOPES TO BE LAMMED AND SEEDED.

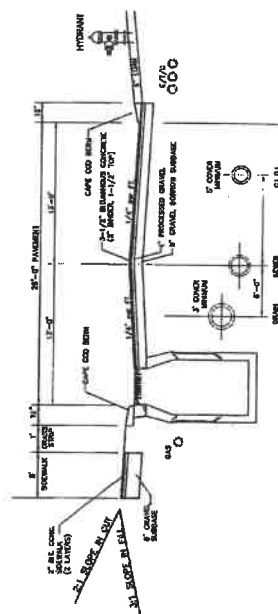






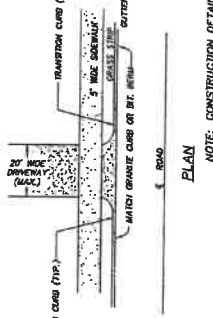
- NOTES:**
1. SIGN SHALL BE IN THE COLOR OF THE SIGNAGE BOARD AS SHOWN IN THE LATEST EDITION OF THE MASSACHUSETTS HIGHWAY DESIGN MANUAL.
  2. ALL MATERIALS SHALL BE THE LATEST FROM MANUFACTURER'S CATALOG.
  3. ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED.
  4. ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED.
  5. ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED.
  6. ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE NOTED.

**TYPICAL STREET SIGN NAME PLATE DETAIL**  
(NO SCALE)



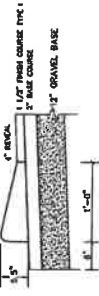
**TYPICAL CROSS SECTION**  
(NO SCALE)

- NOTES:**
1. GRAVEL BROWNSUBBASE SHALL CONFORM TO SECTION 4.02.01 OF THE MASSACHUSETTS HIGHWAY DESIGN MANUAL.
  2. GRAVEL BROWNSUBBASE SHALL BE COMPACTED TO NOT LESS THAN 95 PERCENT OF MAXIMUM DRY DENSITY BY THE STANDARD PROCTOR METHOD.
  3. "TAC TRUCK" AS SPECIFIED IN SECTION 4.02.01 OF THE MASSACHUSETTS HIGHWAY DESIGN MANUAL SHALL BE APPLIED BY THE STANDARD PROCTOR METHOD.
  4. SOILS TESTING INCLUDING SOIL ANALYSIS, PROCTOR TESTING AND COMPACTION TESTING SHALL BE PERFORMED ONCE EVERY 100 FEET OF PAVED ROADWAY ALONG THE CENTERLINE OF THE ROADWAY OR AS DIRECTED BY THE ENGINEER.
  5. SIDEWALK RAMP SHALL MEET CURRENT APPLICABLE ADA / AAS STANDARDS.

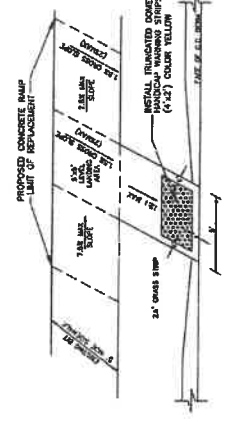


**TYPICAL MINOR DRIVEWAY DETAIL**  
(NO SCALE)

- NOTE:** CONSTRUCTION DETAILS REMAIN THE SAME AS SHOWN IN THE LATEST EDITION OF THE MASSACHUSETTS HIGHWAY DESIGN MANUAL.
- ALL DRIVEWAY APPROACHES TO BE PAVED WITH 3\"/>

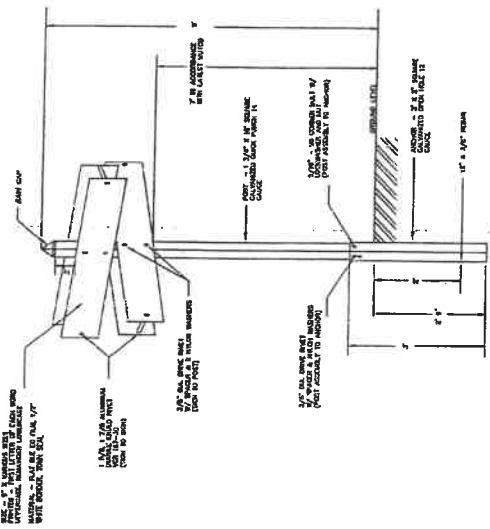


**CAPE COD BERM DETAIL**  
NOT TO SCALE

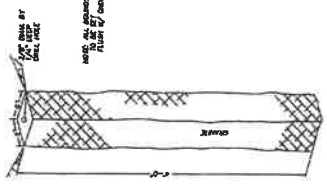


**HANDICAP SIDEWALK RAMP DETAIL**  
(NO SCALE)

- NOTE:** CONSTRUCTION DETAILS REMAIN THE SAME AS SHOWN IN THE LATEST EDITION OF THE MASSACHUSETTS HIGHWAY DESIGN MANUAL.
- ALL DRIVEWAY APPROACHES TO BE PAVED WITH 3\"/>



**TYPICAL STREET SIGN DETAIL**  
(NO SCALE)



**GRANITE MONUMENT DETAIL**  
(NO SCALE)

**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**"UNION SQUARE VILLAGE"**  
**WALPOLE, MASSACHUSETTS**

APPLICANT:  
**WALL STREET DEVELOPMENT CORP.**  
P.O. BOX 272  
WESTYOOD, MA 01980



Town of Walpole  
Date: \_\_\_\_\_  
By: \_\_\_\_\_

APPROVED BY:	DATE:
APPROVED BY:	DATE:
APPROVED BY:	DATE:
APPROVED BY:	DATE:
APPROVED BY:	DATE:

**SHEET: 9 of 12**

**TOWN OF WALPOLE, MASSACHUSETTS**  
**DEPARTMENT OF PUBLIC WORKS**  
**PLAN SHOWING TYPICAL**  
**ROADWAY DETAILS**

REV. #	DESCRIPTION	DATE	BY

- NOTES:**
1. ALL DETAILS ARE SUBJECT TO CHANGE WITHOUT NOTICE.
  2. IT IS THE USER'S SOLE RESPONSIBILITY TO DETERMINE THE SUITABILITY OF A PARTICULAR DETAIL.







NO.	DATE	REVISIONS	DESCRIPTION



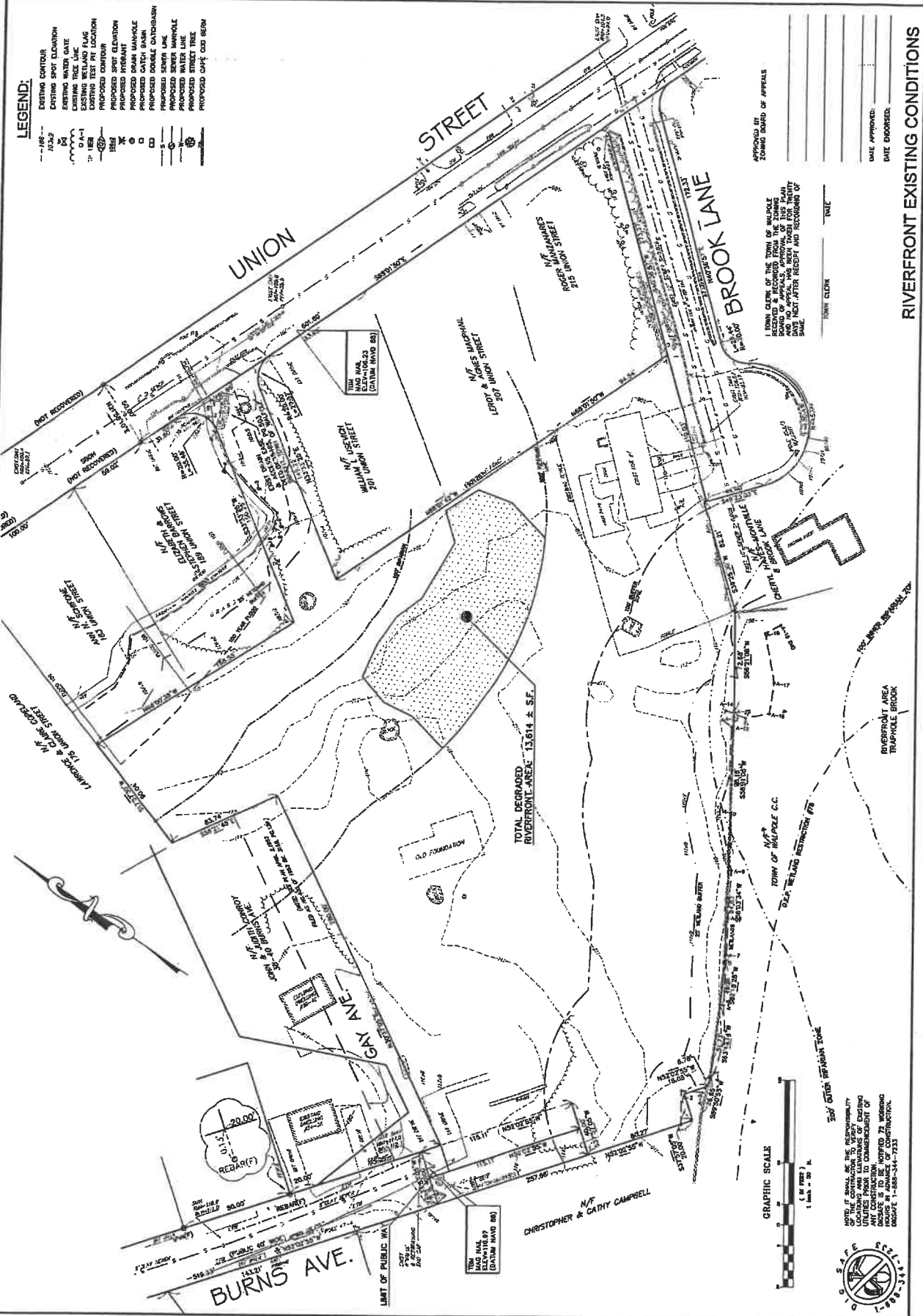
**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
**UNION SQUARE VILLAGE**  
**WALPOLE, MASSACHUSETTS**

APPLICANT:  
 WALL STREET DEVELOPMENT CORP.  
 P.O. BOX 272  
 WESTWOOD, MA 02090

**DESIGNER:**  
 G.M. Construction, Inc.  
 19 EXCHANGE STREET  
 HOLISTON, MA 01746  
 TEL: 508-885-1100  
 FAX: 508-885-1100  
 www.gmconstruction.com

**JOB NO.:** 18,588  
**DATE:** JAN. 24, 2024  
**SCALE:** 1" = 30'  
**SHEET:** SUP-A  
**PLAN #:** 27,096

- LEGEND:**
- EXISTING CONTOUR
  - EXISTING SPOT ELEVATION
  - EXISTING WATER DATE
  - EXISTING WETLAND FLAG
  - EXISTING TEST PIT LOCATION
  - PROPOSED CONTOUR
  - PROPOSED SPOT ELEVATION
  - PROPOSED INFRANT
  - PROPOSED CATCH BASIN
  - PROPOSED DOUBLE CATCH BASIN
  - PROPOSED SEWER LANE
  - PROPOSED SEWER MANHOLE
  - PROPOSED WATER LINE
  - PROPOSED WATER VALVE
  - PROPOSED CHPT. C&T BELOW



APPROVED BY:  
 TOWN CLERK \_\_\_\_\_ TMC

DATE APPROVED: \_\_\_\_\_  
 DATE CHOOSER: \_\_\_\_\_

1. FROM DATE OF THE TIME OF WAIVER RECEIVED & RECORDS FROM THE TOWN BOARD OF APPEALS, APPROVAL OF THIS PLAN SHALL BE VALID FOR A PERIOD OF 30 DAYS FROM THE DATE NEXT AFTER RECESS AND RESUMPTION OF DUTY.

**RIVERFRONT EXISTING CONDITIONS**

**GRAPHIC SCALE**  
 1" = 30'

**NOTE:** IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY EXISTING UTILITIES PRIOR TO COMMENCEMENT OF WORK. THE CONTRACTOR SHALL BE NOTIFIED IN WRITING BY THE TOWN ENGINEER PRIOR TO COMMENCEMENT OF CONSTRUCTION.

DATE: 1/24/24



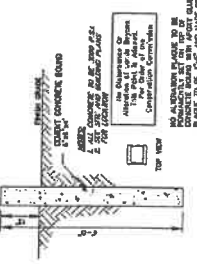


**Proposed 25' Buffer Zone Restoration Area Protocol**  
(9,200 ± sq ft)

1. All soil and other solid waste debris will be removed from the site.
2. The area will be graded to a stable elevation.
3. The area will be covered with a minimum of 6 inches of high quality topsoil.
4. The area will be planted with native shrubs, minimum 1 gallon, container grown, as shown. Plant species will be WCC Staff/consulting list (or other species as approved by WCC Staff):
  - Witch Hazel *Hamamelis virginiana*
  - Hazelnut *Corylus americana*
  - Maple-leaved viburnum *Viburnum acerifolium*
  - Flowering dogwood *Cornus florida*
  - Blackberry *Rubus occidentalis*
5. The area will be planted with New England wetland plants conservation/wildlife mix at 1750 sq/ft or approved substitute; and
6. The area will be watered as necessary during the first six weeks after planting.

**Riverfront Restoration Area (REA) Protocol**

1. Remove and replace surficial degraded soils while retaining the extant trees to the degree feasible.
2. Upon removal of surficial soils, clean topsoil will be placed and spread with minimal care to minimize root damage and soil erosion. The remaining soil will be used, with hand raking to finish the soil surface.
3. Disperse the area with approximately six (6) inches of clean, native leaf litter, either freshly gathered or moderately composted, to mimic the forest floor cover among the extant trees.



**CONSTRUCTION SEQUENCE (RIVERFRONT AREA)**

1. Stake limit of work, install erosion control
2. Cut trees, clear and grub
3. Install sewer line
4. Trench excavation, stockpile material adjacent to trench. Use trench box to stabilize trench.
5. Backfill trench
6. Weir installation
7. Trench excavation, stockpile material adjacent to trench away from river.
8. Backfill trench

TOWN CLERK OF THE TOWN OF WALPOLE  
I HEREBY CERTIFY THAT THE TOWN ENGINEER HAS  
MADE A VISUAL INSPECTION OF THE TOWN PLAN  
AND NO APPEAL HAS BEEN TAKEN FOR AT LEAST  
SIX MONTHS AFTER RECEIPT AND RECORDING OF  
SAME.

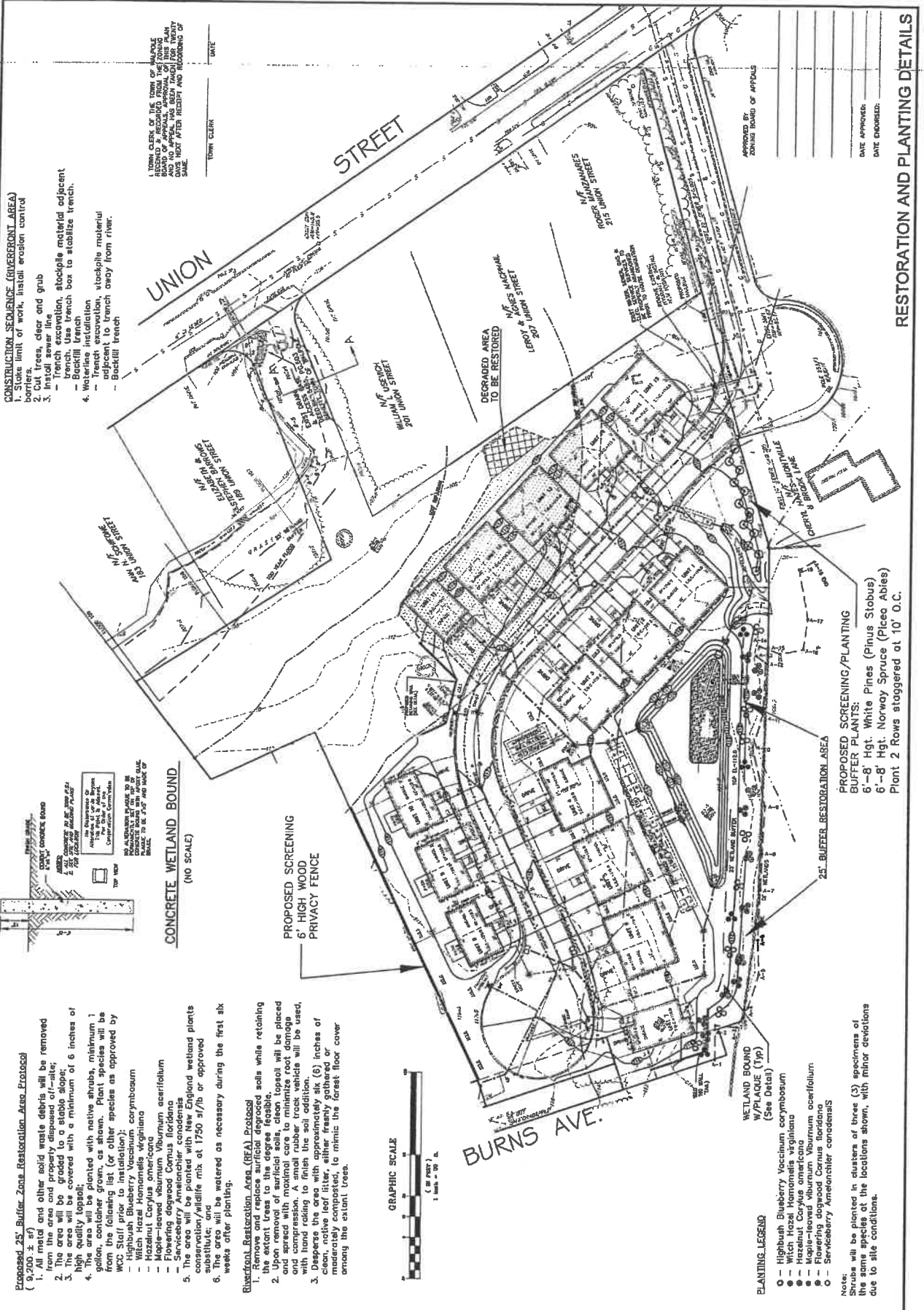


**SPECIAL PERMIT & SITE DEVELOPMENT PLAN**  
"UNION SQUARE VILLAGE"  
WALPOLE, MASSACHUSETTS

APPLICANT:  
WALL STREET DEVELOPMENT CORP.  
P.O. BOX 272  
WESTWOOD, MA 02090

CLM Engineering, Inc.  
100 WASHINGTON STREET  
WALPOLE, MA 01981  
P: 508-428-1100  
F: 508-428-7100  
www.clmengineering.com

JOB NO. 15,698  
DATE: JAN. 24, 2024  
SCALE: 1"= 30'  
SHEET: SUP-C  
PLAN #: 27,096



**PLANTING LEGEND**

- - Highbush Blueberry *Vaccinium corymbosum*
- - Witch Hazel *Hamamelis virginiana*
- - Hazelnut *Corylus americana*
- - Maple-leaved viburnum *Viburnum acerifolium*
- - Flowering dogwood *Cornus florida*
- - Serviceberry *Aamelanchier canadensis*

**Note:**  
Shrubs will be planted in clusters of three (3) specimens of the same species at the locations shown, with minor deviations due to site conditions.

**PROPOSED SCREENING/PLANTING BUFFER PLANTS:**  
6'-8' Hgt. White Pines (*Pinus Stobus*)  
6'-8' Hgt. Norway Spruce (*Picea Abies*)  
Plant 2 Rows staggered at 10' O.C.

**RESTORATION AND PLANTING DETAILS**

# **EXHIBIT II**





February 1, 2023

Mr. Jon Lee, Chairman,  
Zoning Board of Appeals  
Town of Walpole  
135 School Street  
Walpole, MA 02081

Re: Proposed 20-Unit Single Family Home Project  
Brook Lane, Walpole, Traffic Summary Review

Dear Mr. Chairman,

On behalf of the applicant, Wall Street Development Corp., Kimley-Horn and Associates, Inc. (Kimley-Horn) is submitting this traffic summary report for the proposed 20 dwelling-unit, single-family home subdivision off Brook Lane. This traffic summary report follows the guidelines of the Zoning Board of Appeals (ZBA) for traffic analyses of small residential 40B projects.

The project consists of 20 single family homes with access to the development being provided via an extension from Brook Lane. As currently proposed, the street including its extension to Brook Lane, will continue to accommodate two-way travel. The existing cul-de-sac on Brook Lane would be eliminated with this modification resulting in a reduction in paved surface area. The home at 7 Brook Lane would be removed to facilitate the proposed change. The access drive was designed with a 22-foot-wide street with a one-foot cape cod berm on each side that would have a turnaround at its end for large vehicles including fire apparatus. A sidewalk is proposed along one side of the street. The project in total would then have an access point to the area's road network via Brook Lane's intersection with Union Street.

**Union Street in Project Area**

Union Street is classified as an "urban collector" roadway and is owned and maintained by the Town of Walpole. Union Street, a two-way, two lane street, generally follows an east-west alignment connecting Washington Street to the west and Route 1 and the Norwood town line to the east. The alignment is straight and level. The sidewalk on the south side connects Brook Lane with Pleasant Street. The sidewalk is separated from the street by approximately 4 feet of grass. The posted speed limit is 35 mph in the vicinity of Brook Lane.

**Brook Lane**

Brook Lane is a public way with direct access onto Union Street. It is a local residential street that is currently 250 feet in length and ends with a cul-de-sac. The street is 22 feet in width and a sidewalk exists along the eastern side of the street with a berm and small grass strip separating it from the street. It intersects with Union Street on a level, tangent alignment and its approach operates as a STOP controlled leg of the intersection. Brook Lane is approximately 1,100 feet east of Pleasant Street and 1,500 feet west of Route 1. There are four homes directly served by Brook Lane. With the proposed modification, the home at 7 Brook Lane will be eliminated as noted above.



It is expected the future roadway extension of Brook Lane to accommodate the 20-Lot Modification will also be dedicated as a public way. The 20-Lot project calls for the extension of Brook Lane an additional 600 +/- feet. The combined length of the new Brook Lane would be 850 +/- feet. The existing cul-de-sac on Brook Lane will be eliminated as part of this project and the paved area converted to lawn area for the

existing homeowners. The extension of Brook Lane will provide a single-entrance, 40-foot right-of-way from Union Street, including a 22-foot paved travel width with cape-cod berms on each side and one sidewalk.

**Project Related Trip Generation**

As part of this limited traffic review, the anticipated vehicle trip generation of the development was estimated of the project. The proposed project is to construct twenty (20) single family homes. The project will remove one existing single-family home located at 7 Brook Lane to accommodate the road extension from Brook Lane. An estimate of expected trip generation related to the additional project was completed using the latest models and statistics published by the Institute of Transportation Engineers (ITE) in Trip Generation Manual<sup>1</sup> for similar land uses were examined. Trip forecast models are developed by ITE from actual observations and empirical data collected as part of transportation studies. Land Use Code (LUC) 210-Single-Family was selected for this project. The trip forecasts were completed for the proposed development and the results are presented in Table 1. As can be seen, this project is expected to be a relatively low generator of traffic. During the AM peak hour, it is estimated that a total of 17 vehicle trips would be generated with the majority (13) of them exiting the site. During the PM peak hour, it is estimated that 22 vehicle trips will be generated with the majority (14) of these entering trips. The calculation sheets are attached to this letter.

**Table 1 – Summary of Site Trip Generation  
Proposed 20 Single Family Home Subdivision**

	ENTER	EXIT	TOTAL
Weekday 24 Hour	115	115	230
Weekday AM Peak Hour	4	13	17
Weekday PM Peak Hour	14	8	22

ITE LUC 210 Single Family Land Use, 11<sup>th</sup> Edition

The removal of the single family home at 7 Brook Lane to accommodate the road extension would result in a small reduction in the estimates shown in Table 1 including a vehicle trip during each peak hour and up to 12 vehicle trips over the day.

**Sight Distances/Site Drive Visibility**

As part of this review, sight distance analysis was reviewed at the intersection of Union Street with Brook Lane. The minimum criteria for establishing adequate stopping and intersection sight distances are defined by the American Association of State Highway and Transportation Officials (AASHTO).<sup>2</sup> Stopping sight distance (SSD) represents the distance required for a driver traveling at a specified speed to come to a complete stop and therefore relates specifically to safety. Intersection sight distance (ISD) relates to an exiting driver’s view of approaching traffic and represents the distance an approaching vehicle travels during a specified time gap. As indicated by AASHTO, if the available ISD meets or exceeds the minimum SSD criteria, then there is adequate safe sight distance available for motorists to avoid collisions. Minimum required sight distances are calculated based on operating speeds of approaching drivers and the grade of the roadway.

<sup>1</sup> Institute of Transportation Engineers, Trip Generation Manual, 11<sup>th</sup> Edition, Washington, D.C., 2021.

<sup>2</sup> American Association of State Highway Transportation Officials (AASHTO), A Policy on Geometric Design of Highways and Streets, Washington, D.C., 2018.

For 35 mph speeds, the minimum SSD and ISD required for safe movement would be 250 feet. For 40 mph, which exceeds the observed 85<sup>th</sup> percentile speed in the historical data, 305 feet would be required. Field measurements shown in Table 2 have indicated that there is at least 500 feet in both directions available for visibility. Based on this analysis, it is clear that the proposed site drive location is properly situated with respect to safe sight distances. The available sightlines will be more than adequate to ensure safe traffic operations. Visibility along the sidewalk that runs along the south side of Union Street at Brook Lane will remain clearly visible as well.



**Table 2 – Summary of Sight Distance Review**

UNION STREET AT BROOK LANE	SIGHT DISTANCE		
	35 MPH		40 MPH
	MEASURED (FT)	MINIMUM REQUIRED (FT)	MINIMUM REQUIRED (FT)
Union Street Eastbound Approach	500+	250	305
Union Street Westbound Approach	500+	250	305
<b>INTERSECTION SIGHT DISTANCE</b>			
Brook Lane, looking east (Union Street WB traffic)	500+	250	305
Brook Lane, looking west (Union Street EB traffic)	500+	250	305

**Fire Apparatus/Large Vehicle Movement**

With the project as proposed that incorporates a cul-de-sac turnaround, an evaluation of fire truck access was completed. The cul-de-sac has been designed to meet the local design standards. The analysis demonstrated that fire apparatus could access as well as egress the proposed development. These diagrams are attached.

**Conclusion**

As a result of this traffic review for the proposed development to construct 20 homes, it can be concluded that the project will result in a small increase in traffic to the area roadway system. However, to encourage safe traffic movement to and from the project, the following measures are suggested:

- Install a STOP sign and markings on the Brook Lane approach to Union Street.
- Install a NOT A THRU WAY sign on Brook Lane within 150 feet of Union Street,
- Consider installing a raised pedestrian crossing across Brook Lane at Union Street. At minimum, if any modifications are done on the corner roundings at Union Street, the curb ramps will need to be reconstructed to be ADA compliant.

Page 4 of 4

All traffic control signage and markings should conform to the MUTCD<sup>3</sup>.

If you have any questions or need additional information, please do not hesitate to contact me at 508-395-3334.

Very truly yours,  
**KIMLEY-HORN & ASSOCIATES, INC.**

*William J Scully*

William J. Scully, P.E.  
(MA 33298)

WJS/-

Attachments

Trip Generation Calculation Sheets  
Fire Truck Movement diagrams  
Historical Traffic and Speed Data

---

<sup>3</sup> U.S. Department of Transportation, Federal Highway Administration, Manual on Uniform Traffic Control Devices (MUTCD), Washington, D.C., 2009.



PRECISION  
D A T A  
INDUSTRIES LLC

49 Morton Street, Framingham, MA 01702  
Office: 508-873-0100 Fax: 508-873-0118  
Email: datarequest@pdinc.com

Union Street  
Just west of Brook Lane  
City, State: Walpole, MA  
Client: Green International / J. Gauvin  
EB

165334 A Speed  
Site Code: 16082

Start Time	14	15	18	20	24	25	29	30	34	35	39	40	44	45	49	50	54	55	59	60	64	65	69	70	9999	Total	85th %ile	Ave Speed
10/26/16	0	0	0	0	2	1	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	40	36	
01:00	0	0	0	0	0	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	37	35	
02:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	37	34	
03:00	0	0	0	0	2	4	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	41	35	
04:00	0	0	0	0	5	8	7	6	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	30	43	36	
05:00	0	0	0	1	6	18	27	18	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	72	41	36	
06:00	0	0	4	13	47	46	17	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	130	39	35	
07:00	3	1	12	24	65	53	15	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	177	38	33	
08:00	0	1	2	19	63	91	26	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	203	38	35	
09:00	0	1	1	27	68	64	24	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	178	39	34	
10:00	0	1	4	10	45	66	23	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	156	40	36	
11:00	0	1	4	19	60	58	26	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	182	40	35	
12 PM	0	0	1	19	68	90	23	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	205	38	35	
13:00	0	1	5	12	58	65	17	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	159	38	34	
14:00	1	2	5	16	74	65	24	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	192	39	34	
15:00	2	3	3	18	84	83	20	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	195	38	34	
16:00	0	0	5	19	58	94	35	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	212	39	35	
17:00	0	5	4	10	77	91	23	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	216	38	35	
18:00	0	0	6	11	62	64	14	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	149	38	35	
19:00	0	2	3	9	28	41	15	2	1	1	0	1	0	1	0	1	0	0	0	0	0	0	0	0	103	40	35	
20:00	0	1	2	6	14	20	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	54	40	35	
21:00	1	0	0	3	12	10	6	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	34	41	35	
22:00	1	1	0	0	8	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	38	33	
23:00	0	0	0	2	5	7	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15	38	35	
Total	8	20	62	252	882	1063	347	49	6	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2694			
%	0.3%	0.7%	2.3%	9.4%	32.7%	39.5%	12.9%	1.8%	0.2%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
AM Peak	07:00	07:00	07:00	09:00	07:00	08:00	08:00	10:00	04:00																	08:00		
Vol.	3	1	12	27	85	91	25	7	1																	203		
PM Peak	15:00	17:00	18:00	12:00	17:00	16:00	16:00	14:00	17:00	18:00																17:00		
Vol.	2	5	6	19	77	94	35	5	2	1																216		

Stats

- 15th Percentile : 29 MPH
- 50th Percentile : 34 MPH
- 85th Percentile : 39 MPH
- 85th Percentile : 42 MPH
- Mean Speed(Average) : 35 MPH
- 10 MPH Pace Speed : 30-39 MPH
- Number In Pace : 1845
- Percent In Pace : 72.2%
- Number of Vehicles > 35 MPH : 1257
- Percent of Vehicles > 35 MPH : 46.7%



PRECISION  
D A T A  
INDUSTRIES LLC

46 Morton Street, Framingham, MA 01702  
 Office: 508-475-0100 Fax: 508-475-0118  
 Email: datarequests@pdilc.com

Union Street  
 Just west of Brook Lane  
 City, State: Walpole, MA  
 Client: Green International / J. Gauvin  
 EB

165334 A Speed  
 Site Code: 16082

Start Time	14	15	20	25	30	35	40	45	50	65	60	65	70	Total	85th %ile	Ave Speed
10/27/16	0	0	3	0	2	1	2	0	0	0	0	0	0	8	41	31
01:00	0	0	0	0	2	2	1	0	0	0	0	0	0	5	40	35
02:00	0	0	0	0	1	2	0	0	0	0	0	0	0	3	37	35
03:00	0	0	0	1	2	2	1	1	0	0	0	0	0	7	43	36
04:00	0	0	0	3	8	10	4	1	2	0	0	0	0	29	42	36
05:00	0	0	0	1	10	31	18	2	0	0	0	0	0	62	41	38
06:00	0	0	1	22	37	32	14	2	0	0	0	0	0	108	38	34
07:00	1	6	9	17	65	68	13	4	0	0	0	0	0	183	38	33
08:00	15	18	10	18	59	59	17	1	0	0	0	0	0	207	38	31
09:00	8	10	25	32	58	34	16	2	0	0	0	0	0	185	37	30
10:00	0	2	8	22	50	44	17	1	0	0	0	0	0	145	38	33
11:00	0	1	3	24	72	69	23	1	0	0	0	0	0	193	38	34
12 PM	1	0	4	21	69	68	17	5	0	0	0	0	0	185	38	34
13:00	0	1	5	18	58	65	17	0	0	0	0	0	0	162	38	34
14:00	3	1	5	13	71	89	17	1	0	0	0	0	0	200	38	34
15:00	2	1	8	23	69	68	23	3	0	0	0	0	0	197	38	34
16:00	0	2	4	12	70	72	18	1	0	0	0	0	0	177	38	34
17:00	0	1	13	24	78	71	9	2	0	0	0	0	0	198	37	33
18:00	0	0	6	28	58	46	1	1	0	0	0	0	0	140	36	32
19:00	1	1	5	11	30	35	7	2	0	0	0	0	0	82	38	33
20:00	1	0	1	10	22	13	5	0	0	0	0	0	0	52	37	33
21:00	0	0	4	6	10	16	5	2	0	0	0	0	0	42	39	34
22:00	2	0	0	2	6	9	3	1	0	0	0	0	0	23	39	33
23:00	0	0	0	2	3	8	2	0	0	0	0	0	0	15	38	35
Total	34	44	115	309	909	824	248	33	2	0	0	0	0	2618		
%	1.3%	1.7%	4.4%	11.8%	34.7%	35.3%	9.5%	1.3%	0.1%	0.0%	0.0%	0.0%	0.0%			
AM Peak	08:00	08:00	09:00	09:00	11:00	08:00	11:00	07:00	04:00					08:00		
Vol.	15	18	25	32	72	69	23	4	2					207		
PM Peak	14:00	16:00	17:00	18:00	17:00	14:00	15:00	12:00						14:00		
Vol.	3	2	13	28	78	89	23	5						200		

Stats

- 15th Percentile : 27 MPH
- 50th Percentile : 33 MPH
- 85th Percentile : 38 MPH
- 95th Percentile : 42 MPH

Mean Speed(Average) : 33 MPH

10 MPH Pace Speed : 30-39 MPH

- Number in Pace : 1833
- Percent in Pace : 70.0%

Number of Vehicles > 35 MPH : 1022

Percent of Vehicles > 35 MPH : 39.0%



PRECISION  
D A T A  
INDUSTRIAL

44 Morton Street, Framingham, MA 01702  
Office: 508-875-0100 Fax: 508-875-0111  
Email: datarequests@pdic.com

Union Street  
Just west of Brook Lane  
City, State: Walpole, MA  
Client: Green International / J. Gauvin

165334 A Speed  
Site Code: 16082

WB	Start Time	1	15	20	25	30	35	40	45	50	55	60	65	70	Total	85th %ile	Ave Speed
	10/26/	14	19	24	29	34	39	44	49	54	59	64	69	9999			
	16	0	0	0	0	7	4	1	1	0	0	0	0	0	13	39	35
	01:00	0	0	0	0	4	2	0	0	0	0	0	0	0	6	36	34
	02:00	0	0	2	1	3	0	0	2	0	0	0	0	0	8	46	33
	03:00	0	0	3	3	1	3	2	0	0	0	0	0	0	12	39	31
	04:00	0	0	1	4	4	3	4	0	0	0	0	0	0	16	41	34
	05:00	0	2	3	13	9	21	4	1	0	0	0	0	0	53	38	33
	06:00	0	0	4	34	62	45	6	1	0	0	0	0	0	152	37	33
	07:00	0	1	6	43	144	70	11	0	1	0	0	0	0	276	36	33
	08:00	0	1	2	40	96	85	16	1	1	0	0	0	0	242	37	34
	09:00	1	3	3	39	87	63	8	0	0	0	0	0	0	204	37	32
	10:00	0	2	3	20	80	69	13	2	0	0	0	0	0	189	38	34
	11:00	0	0	4	20	106	72	10	2	0	0	0	0	0	214	37	34
	12 PM	0	1	6	31	123	83	7	0	0	0	0	0	0	251	37	33
	13:00	0	0	3	61	128	79	10	0	0	0	0	0	0	281	36	33
	14:00	0	1	4	32	99	82	16	0	0	0	0	0	0	234	37	34
	15:00	0	2	7	44	108	76	18	4	0	1	0	0	1	281	37	33
	16:00	0	0	2	25	114	108	15	5	0	0	0	0	0	269	38	34
	17:00	0	0	5	36	174	116	19	1	0	0	0	0	0	351	37	34
	18:00	0	2	18	27	117	80	16	1	1	0	0	0	0	262	37	33
	19:00	1	2	2	16	83	71	13	1	0	0	0	0	0	188	38	34
	20:00	0	1	3	13	46	39	13	4	2	0	0	0	0	121	39	35
	21:00	1	0	0	5	47	25	8	1	0	0	0	0	0	87	38	34
	22:00	1	1	1	6	17	23	5	0	0	0	0	0	0	53	38	34
	23:00	0	0	0	3	5	10	5	0	2	0	0	0	0	25	42	37
	Total	4	19	82	514	1664	1229	220	27	7	1	0	0	1	3768		
	%	0.1%	0.5%	2.2%	13.6%	44.2%	32.6%	5.8%	0.7%	0.2%	0.0%	0.0%	0.0%	0.0%			
AM Peak	09:00	09:00	07:00	07:00	07:00	08:00	08:00	02:00	07:00						07:00		
Vol.	1	3	6	43	144	85	16	2	1						276		
PM Peak	19:00	15:00	18:00	13:00	17:00	17:00	17:00	16:00	20:00	15:00				15:00	17:00		
Vol.	1	2	18	61	174	116	19	5	2	1				1	351		

Stats

15th Percentile : 28 MPH  
50th Percentile : 32 MPH  
85th Percentile : 37 MPH  
95th Percentile : 40 MPH

Mean Speed(Average) : 33 MPH  
10 MPH Pace Speed : 30-39 MPH  
Number In Pace : 2893  
Percent In Pace : 76.8%  
Number of Vehicles > 35 MPH : 1239  
Percent of Vehicles > 35 MPH : 32.9%



PRECISION  
DATA  
INDUSTRIES LLC

44 Horton Street, Fitchburg, MA 01702  
Office: 508-873-0100 Fax: 508-873-0110  
Email: datarequests@pdillc.com

Union Street  
Just west of Brook Lane  
City, State: Walpole, MA  
Client: Green International / J. Gauvin

165334 A Speed  
Site Code: 16082

WB	1	15	20	25	30	35	40	45	50	55	60	65	70	Total	85th %ile	Ave Speed
Start Time	14	19	24	29	34	39	44	49	54	59	64	69	9999			
10/27/16	0	0	0	1	3	3	3	1	0	0	0	0	0	11	42	37
01:00	0	0	0	0	3	3	1	0	0	0	0	0	0	7	38	36
02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	1	28	27
03:00	0	0	2	0	1	4	1	0	0	0	0	0	0	8	38	33
04:00	0	0	1	2	7	4	0	0	0	0	0	0	0	14	36	32
05:00	0	1	1	10	18	11	5	0	0	0	0	0	0	46	38	33
06:00	0	0	2	29	78	54	8	2	0	0	0	0	0	173	37	33
07:00	0	0	10	42	132	73	16	1	0	0	0	0	0	274	37	33
08:00	4	28	31	50	95	46	9	0	0	0	0	0	0	263	35	29
09:00	1	0	7	56	72	30	9	0	1	0	0	0	0	176	36	31
10:00	3	8	27	38	55	49	8	0	0	0	0	0	0	188	36	30
11:00	3	1	9	34	92	71	12	1	0	1	0	0	0	224	37	33
12 PM	0	3	8	35	132	79	7	0	0	0	0	0	0	262	36	33
13:00	1	1	6	36	108	71	13	0	0	0	0	0	0	236	37	33
14:00	0	1	2	26	125	83	24	2	0	0	0	0	0	263	38	34
15:00	1	1	4	33	90	89	22	1	0	0	0	0	0	242	38	34
16:00	0	0	2	38	119	82	15	2	0	0	0	0	0	258	37	33
17:00	0	1	11	58	158	73	4	1	0	0	0	0	0	304	36	32
18:00	1	1	13	77	120	40	4	2	0	0	0	0	0	258	34	31
19:00	3	0	3	28	79	48	11	1	0	0	0	0	0	173	37	33
20:00	0	1	1	13	71	55	10	0	0	1	0	0	0	152	37	34
21:00	0	0	1	14	36	31	4	1	0	0	0	0	0	87	37	33
22:00	0	0	0	6	19	17	5	1	0	0	0	0	0	48	38	34
23:00	0	0	0	1	6	11	5	0	1	0	0	0	0	23	41	37
Total	17	47	139	628	1816	1028	196	16	2	2	0	0	0	3691		
%	0.5%	1.3%	3.8%	17.0%	43.8%	27.9%	5.3%	0.4%	0.1%	0.1%	0.0%	0.0%	0.0%			
AM Peak	08:00	08:00	08:00	09:00	07:00	07:00	07:00	08:00	09:00	11:00				07:00		
Vol.	4	28	31	56	132	73	16	2	1	1				274		
PM Peak	19:00	12:00	18:00	18:00	17:00	15:00	14:00	14:00	23:00	20:00				17:00		
Vol.	3	3	13	77	158	90	24	2	1	1				304		

Stats

15th Percentile : 28 MPH  
50th Percentile : 32 MPH  
85th Percentile : 37 MPH  
95th Percentile : 39 MPH

Mean Speed(Average) : 32 MPH  
10 MPH Pace Speed : 30-39 MPH  
Number In Pace : 2644  
Percent In Pace : 71.6%  
Number of Vehicles > 35 MPH : 1038  
Percent of Vehicles > 35 MPH : 28.1%



# Single-Family Detached Housing (210)

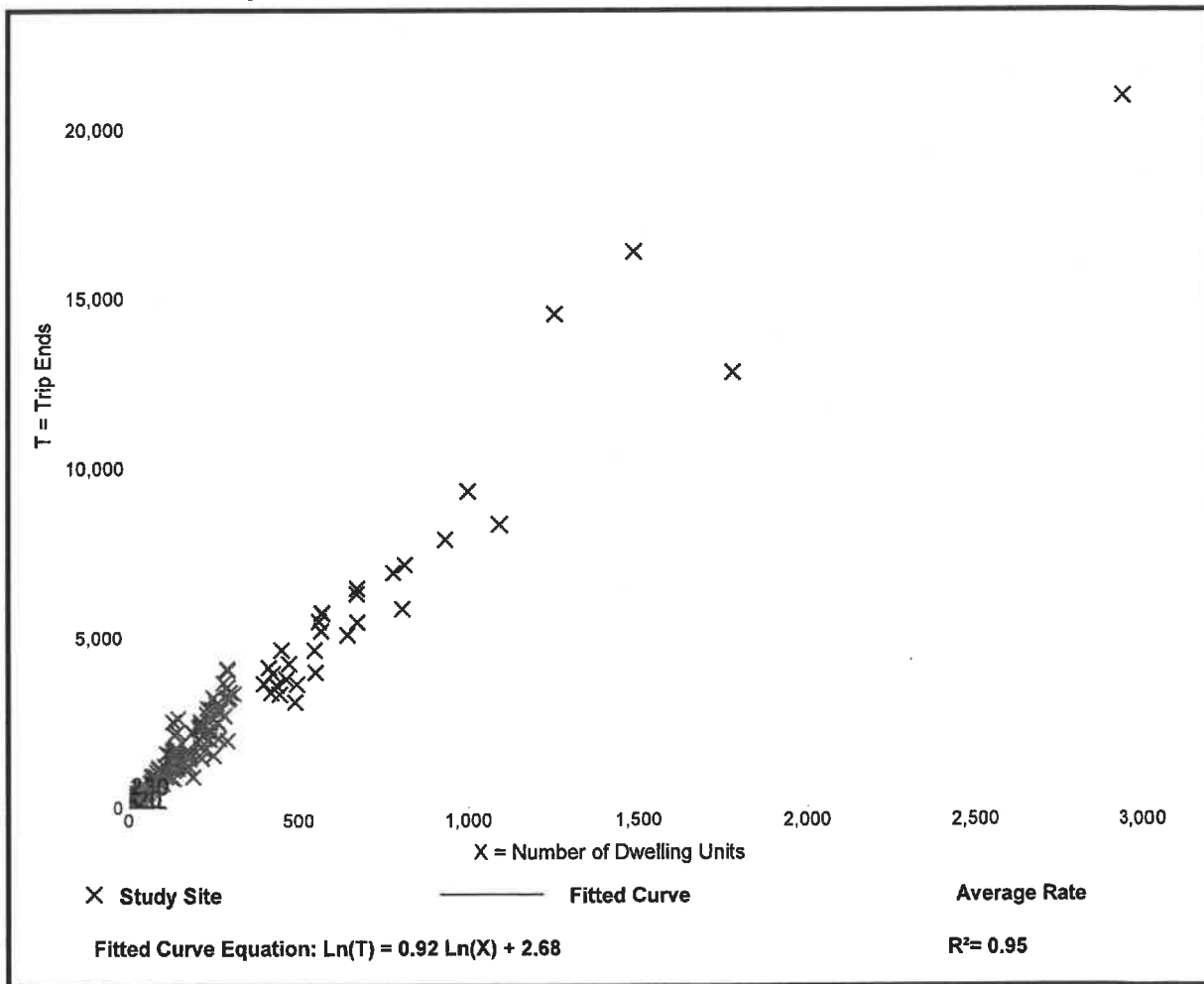
**Vehicle Trip Ends vs: Dwelling Units**  
On a: **Weekday**

**Setting/Location: General Urban/Suburban**  
Number of Studies: 174  
Avg. Num. of Dwelling Units: 246  
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
9.43	4.45 - 22.61	2.13

## Data Plot and Equation



# Single-Family Detached Housing (210)

**Vehicle Trip Ends vs: Dwelling Units**

**On a: Weekday,  
Peak Hour of Adjacent Street Traffic,  
One Hour Between 7 and 9 a.m.**

**Setting/Location: General Urban/Suburban**

Number of Studies: 192

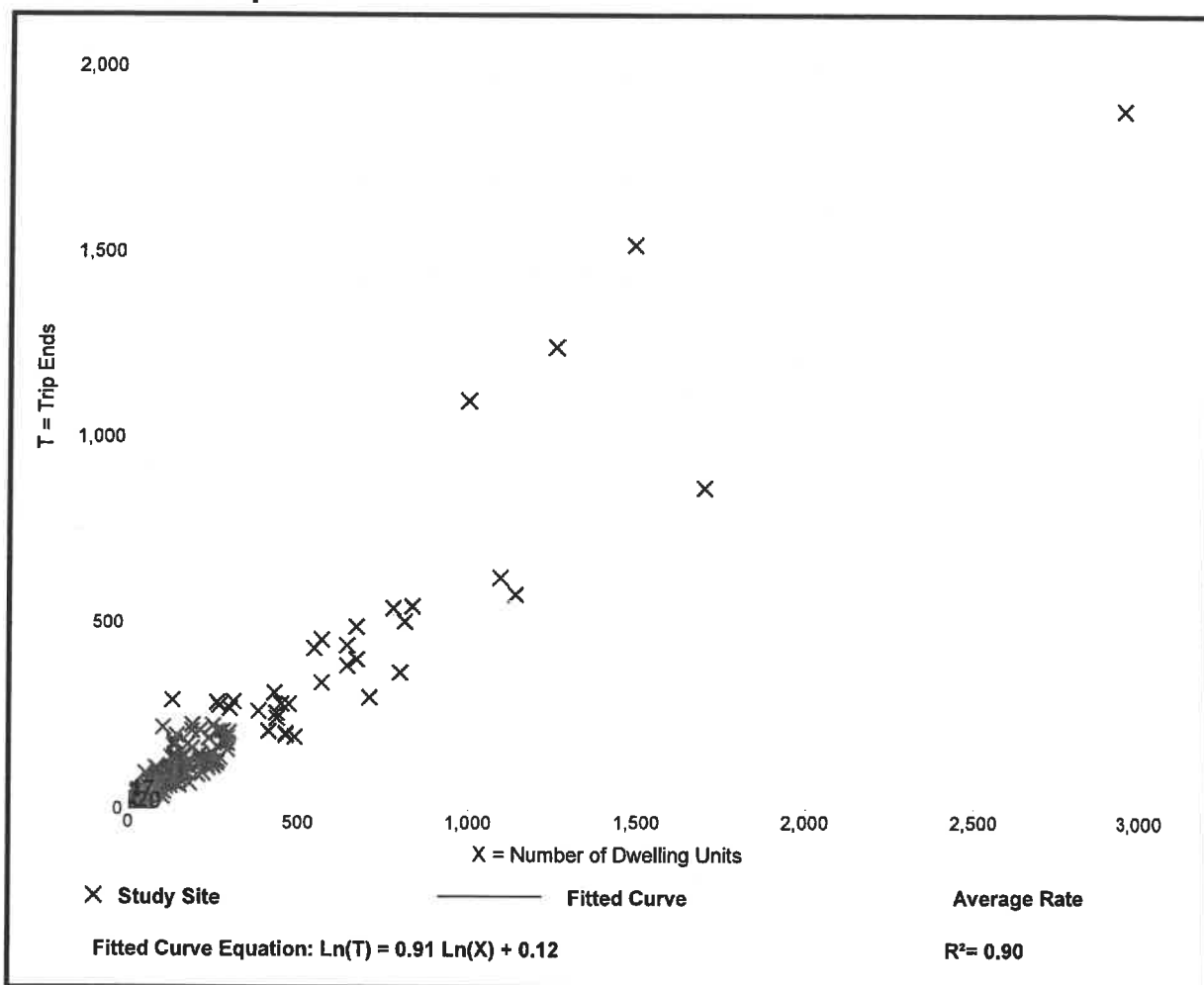
Avg. Num. of Dwelling Units: 226

Directional Distribution: 25% entering, 75% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.70	0.27 - 2.27	0.24

## Data Plot and Equation



# Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,  
Peak Hour of Adjacent Street Traffic,  
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 208

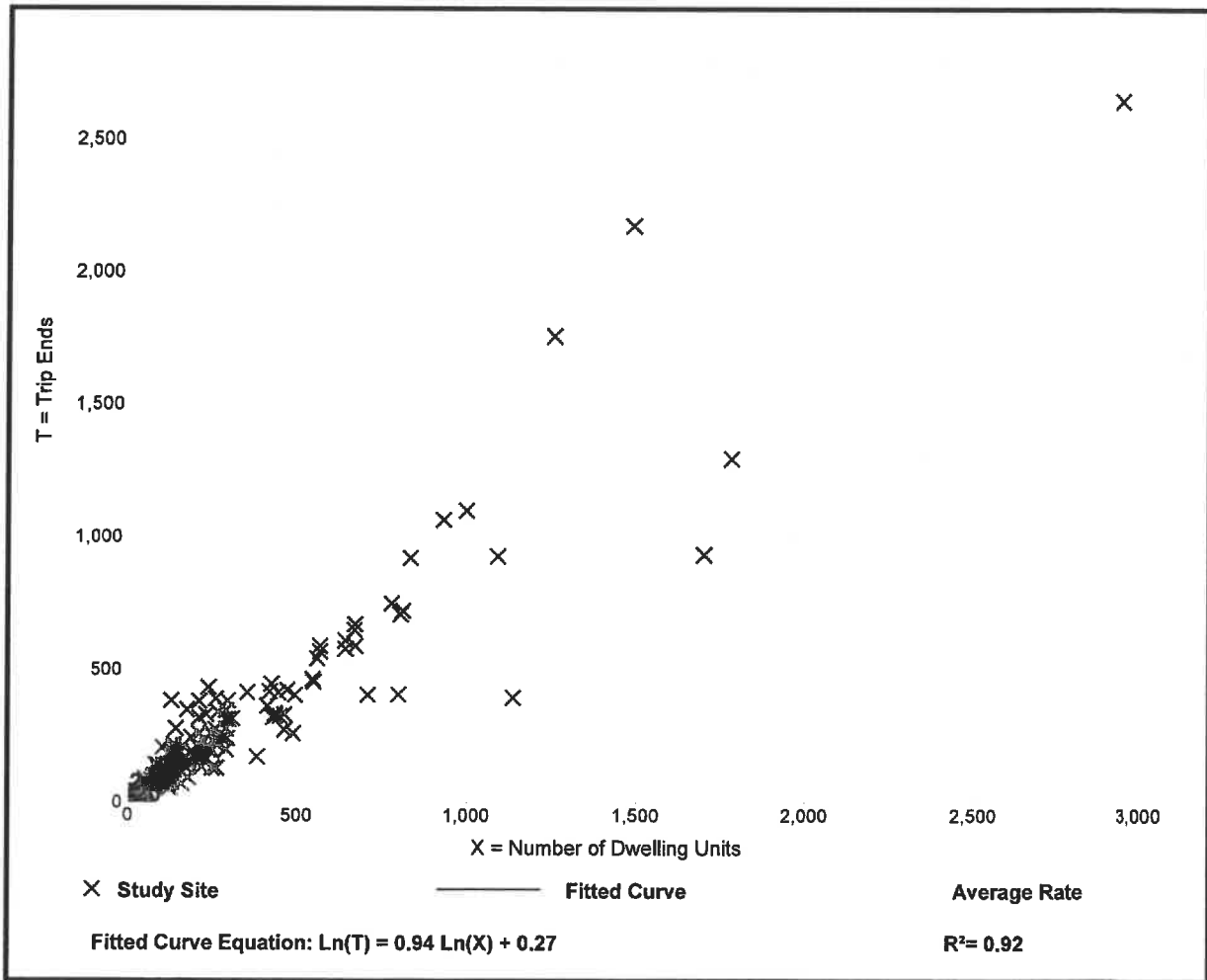
Avg. Num. of Dwelling Units: 248

Directional Distribution: 63% entering, 37% exiting

## Vehicle Trip Generation per Dwelling Unit

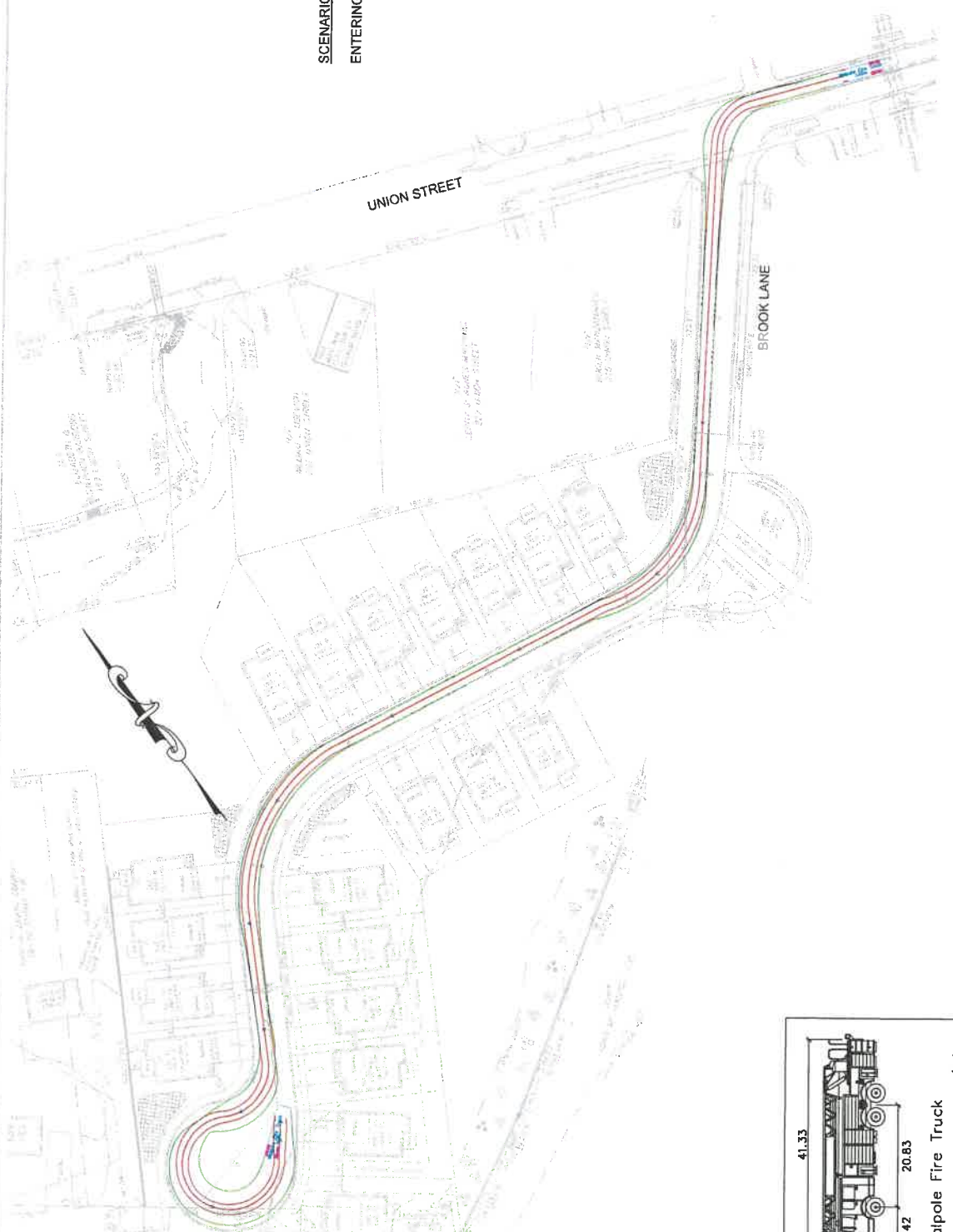
Average Rate	Range of Rates	Standard Deviation
0.94	0.35 - 2.98	0.31

## Data Plot and Equation

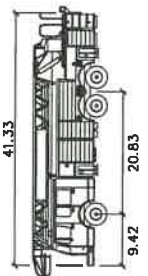




**SCENARIO 1**  
 ENTERING FROM EAST



REFERENCE:  
 **GLM** Engineering Consultants, Inc.  
 AMENDED SITE DEVELOPMENT PLAN  
 A COMPREHENSIVE PERMIT M.G.L.c. 40B



**Waipole Fire Truck**

Width	: 8.33
Wheelbase	: 9.42
Lock to Lock Time	: 6.0
Steering Angle	: 42.0

feet

**TURNING MOMENTS LEGEND:**  
 THE LINES  
 FRONT BUMPER LINES

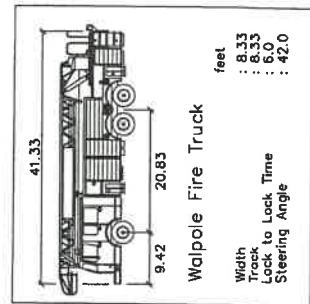
BURNS AVE.



**SCENARIO 2**  
 EXITING FROM WEST



BURNS AVE.

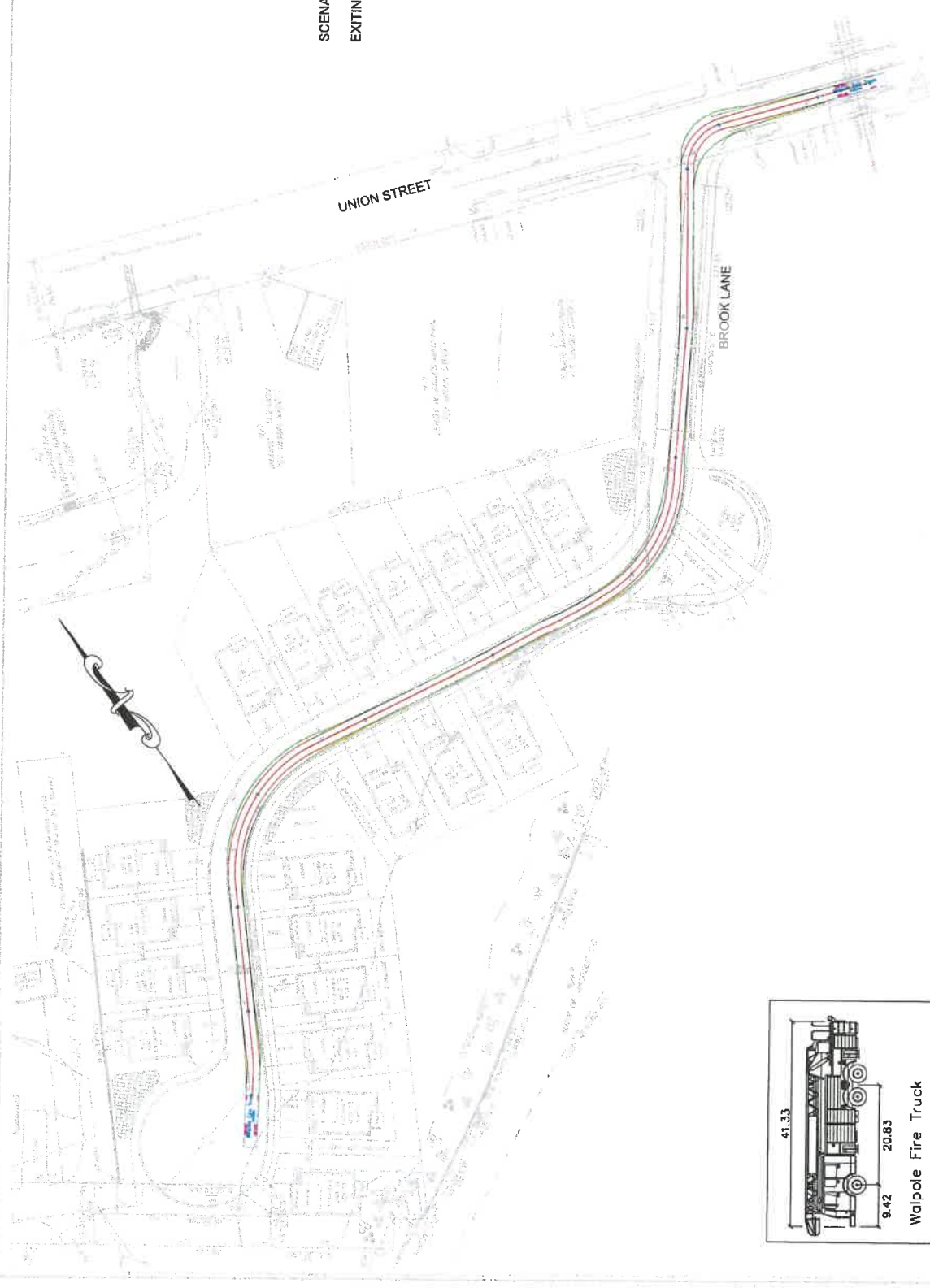


**TURNING MOMENTS LEGEND:**  
 TIRE LINES  
 FRONT BUMPER LINES

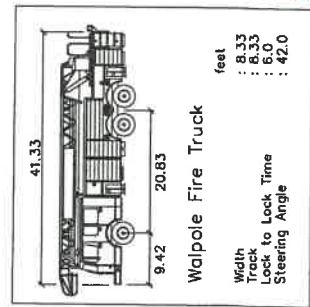
REFERENCE:  
 GLM Engineering Consultants, Inc.  
 AMENDED SITE DEVELOPMENT PLAN  
 A COMPREHENSIVE PERMIT M.G.L.c. 40B



SCENARIO 3  
 EXITING TO EAST



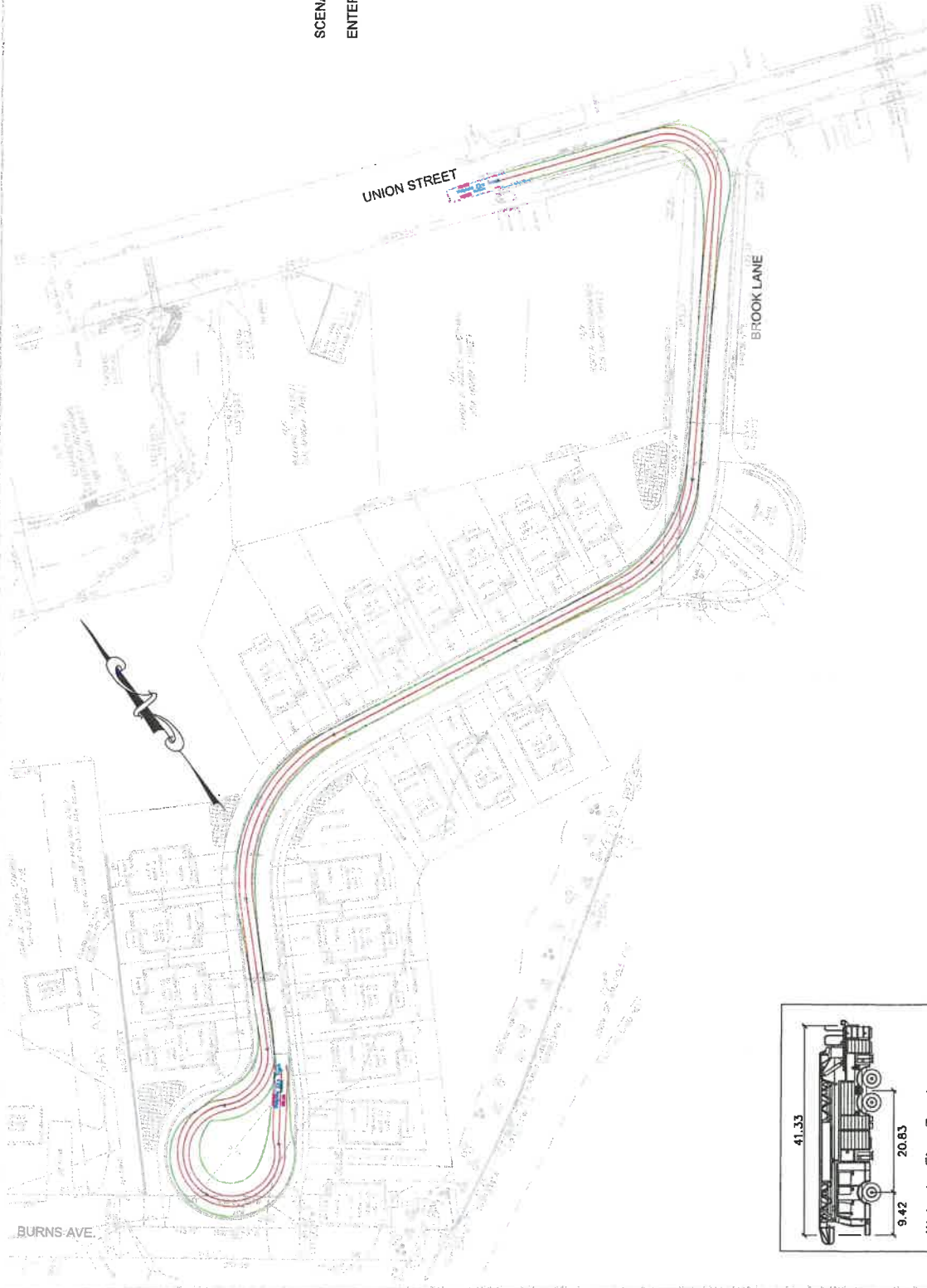
REFERENCE:  
 GLM Engineering Consultants, Inc.  
 AMENDED SITE DEVELOPMENT PLAN  
 A COMPREHENSIVE PERMIT M.G.L.c. 40B



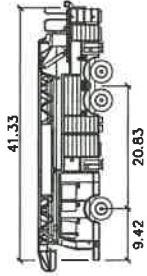
TURNING MOMENTS LEGEND:  
 --- TIRE LINES  
 --- FRONT BUMPER LINES



SCENARIO 4  
 ENTERING FROM WEST



REFERENCE:  
 Engineering  
 Consultants, Inc.  
 AMENDED SITE DEVELOPMENT PLAN  
 A COMPREHENSIVE PERMIT M.G.L.c. 40B



**Walpole Fire Truck**

Width	: 41.33	feet
Track	: 9.42	
Lock to Lock Time	: 20.83	
Steering Angle	: 8.33	
	: 6.0	
	: 42.0	

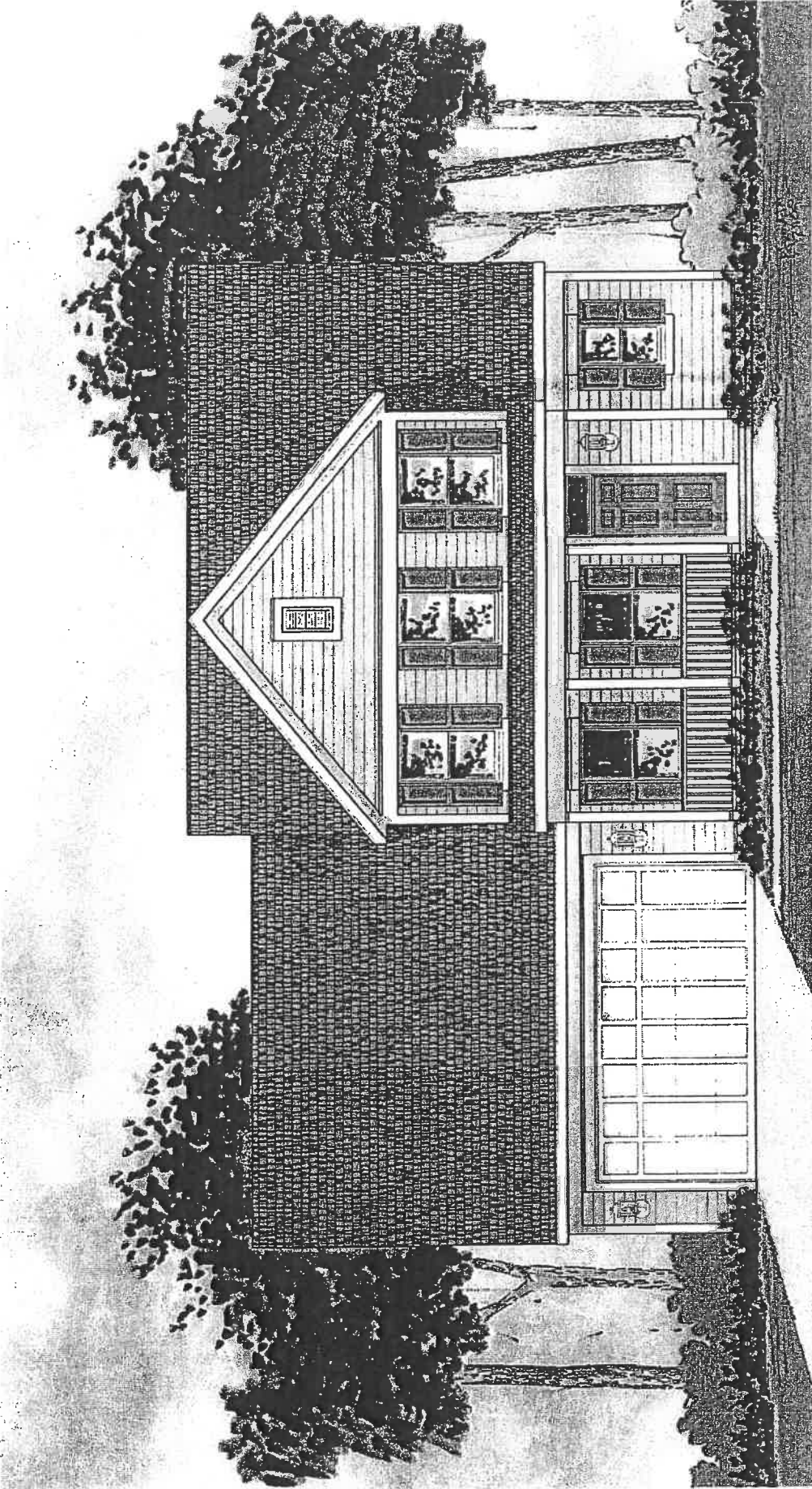
**TURNING MOMENTS LEGEND:**

- TIRE LINES
- FRONT BUMPER LINES

BURNS AVE

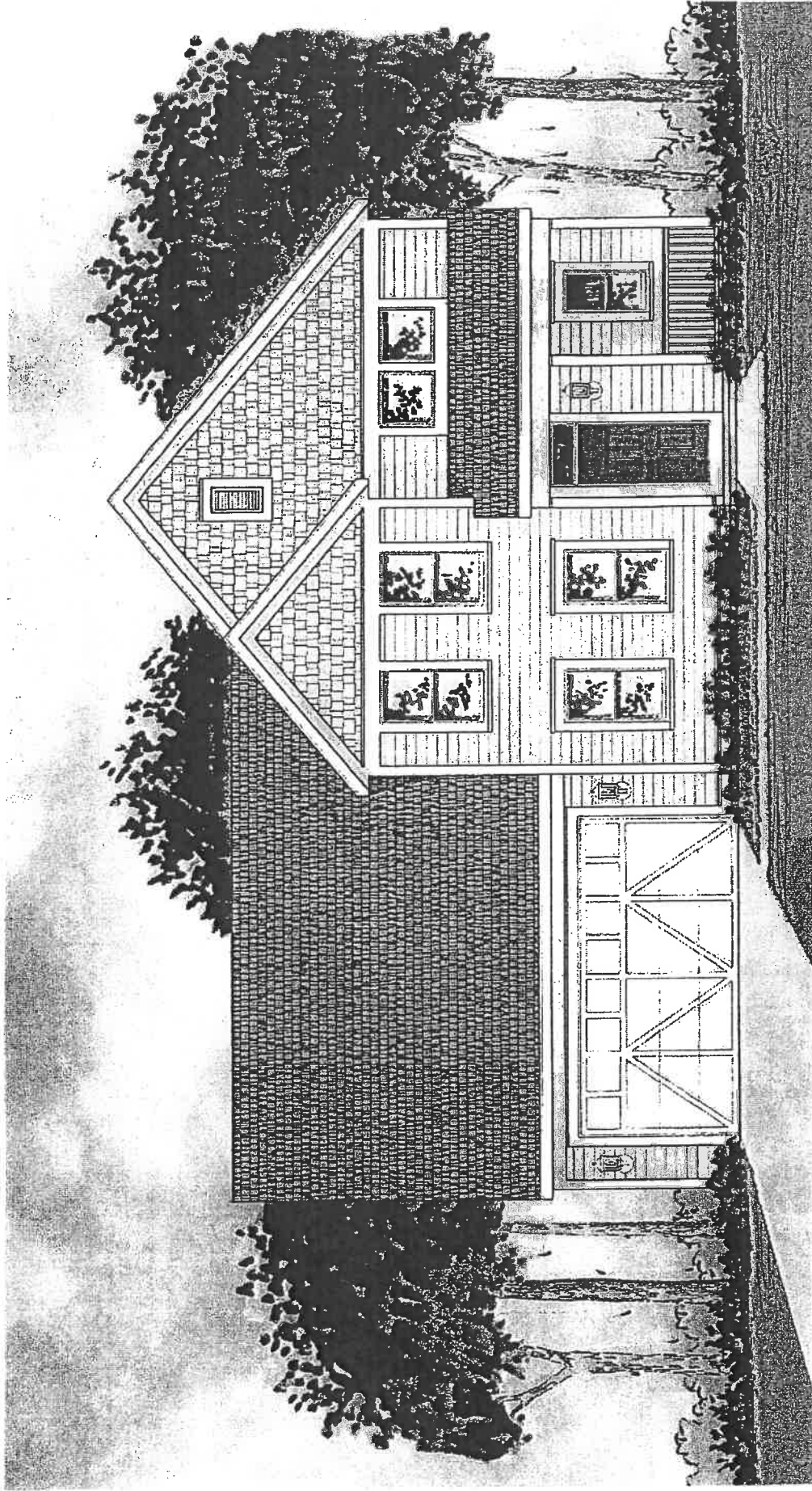
# **EXHIBIT III**





**PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.**

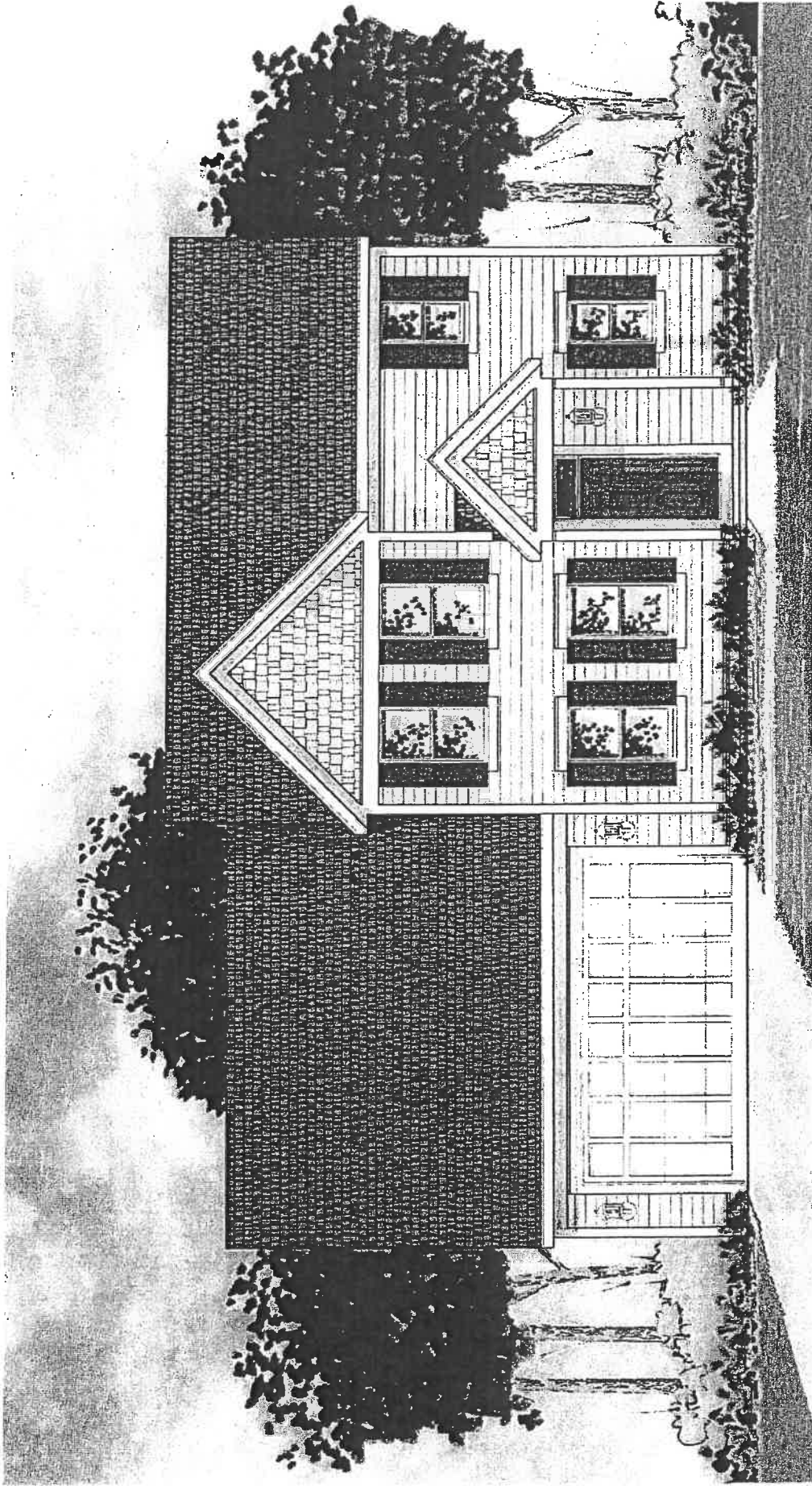
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**PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.**

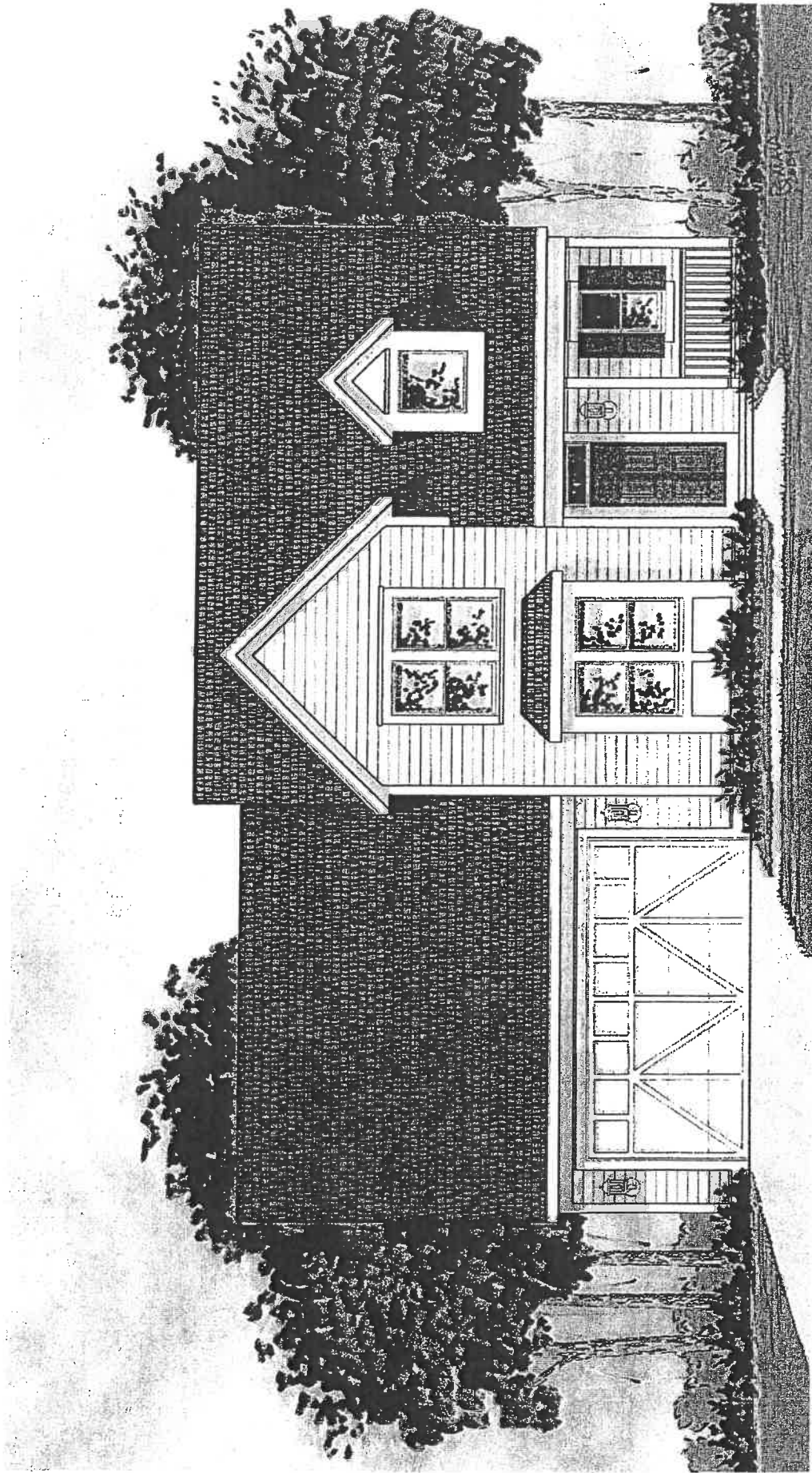
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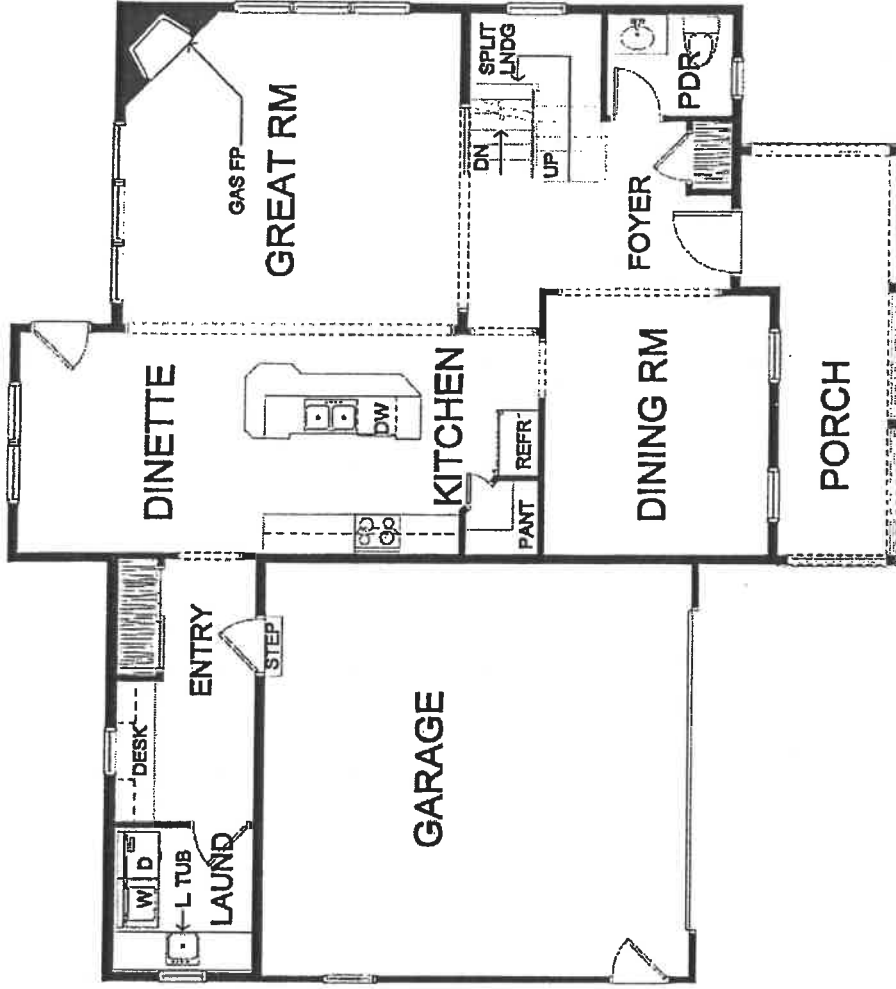




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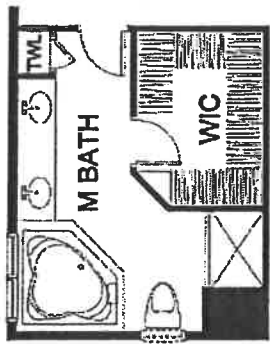
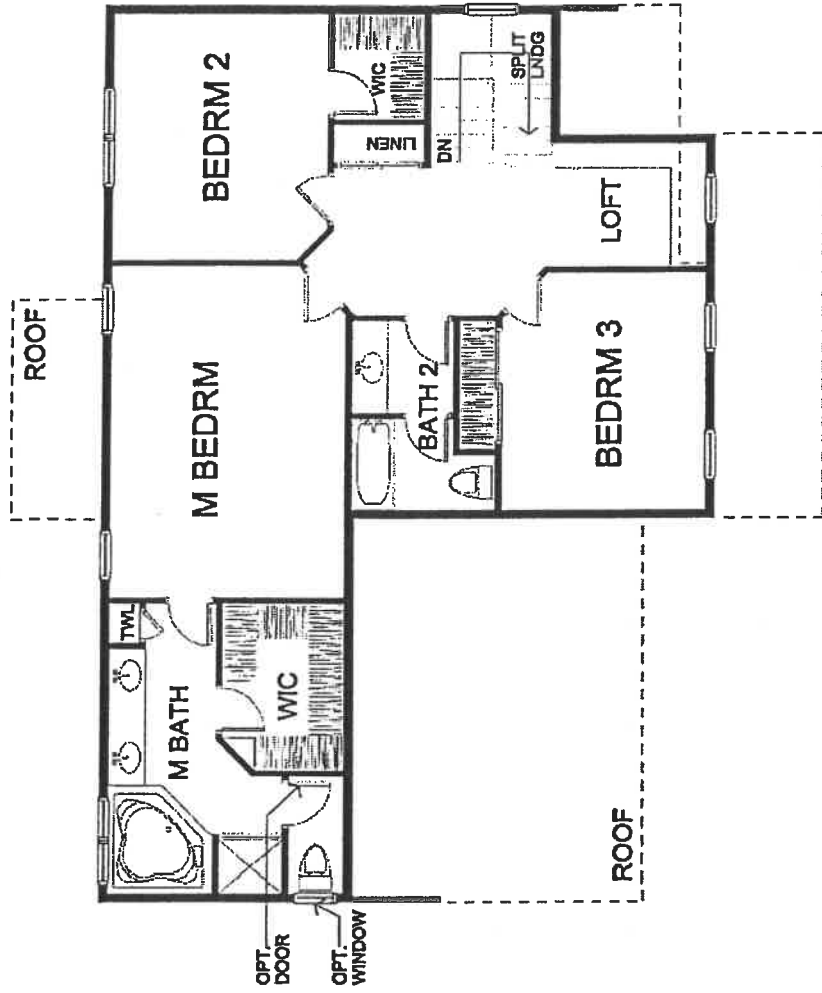
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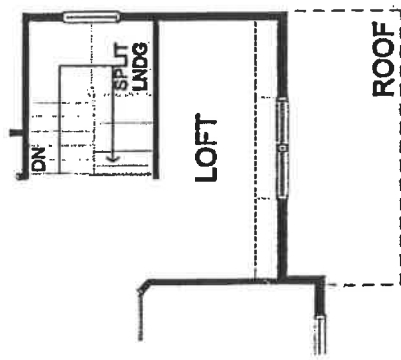


**FIRST FLOOR**  
1152 SQ FT

**PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.**



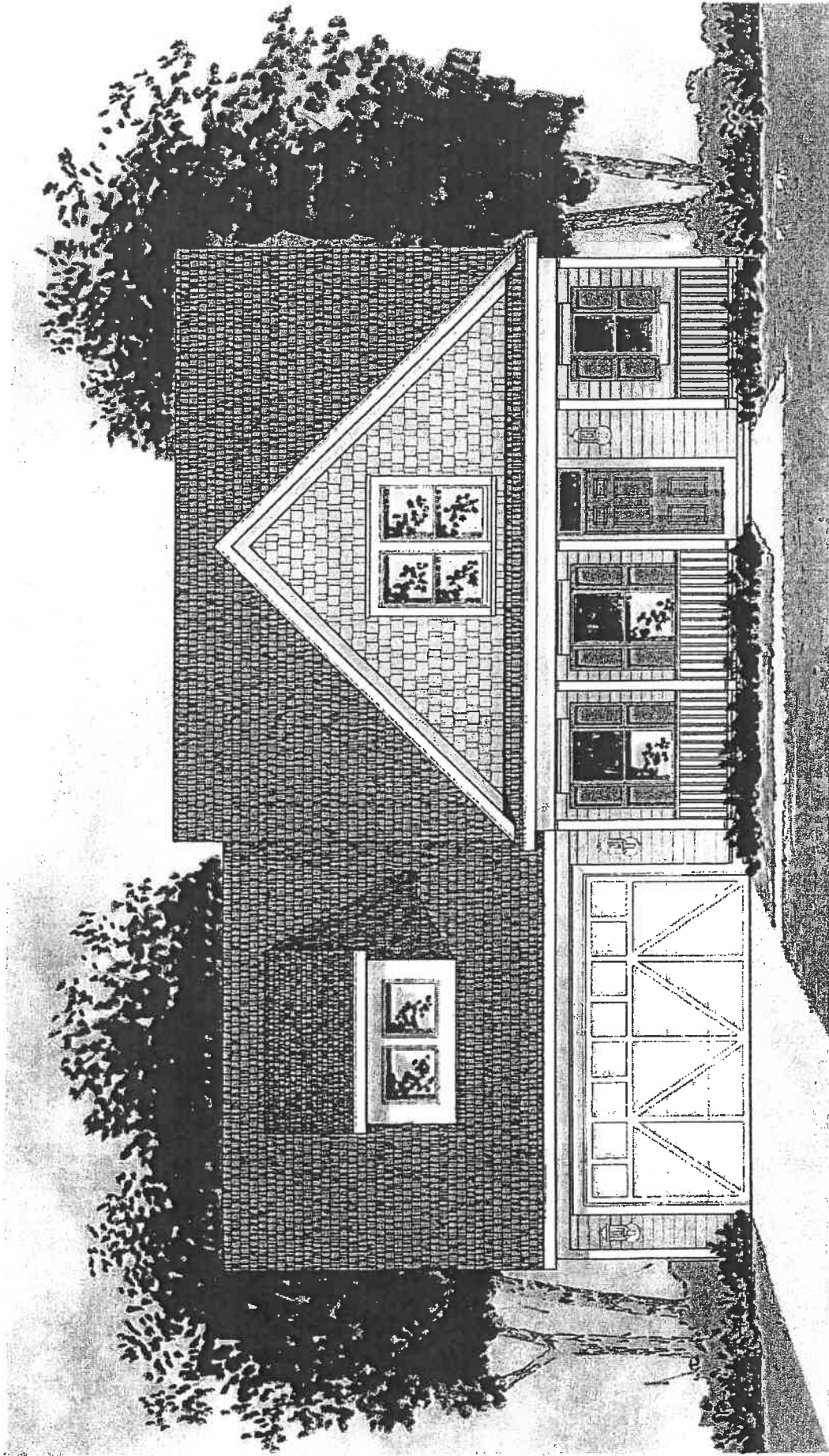
ALT. M. BATH



ALT. LOFT

**SECOND FLOOR**  
1124 SQ FT

**PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.**



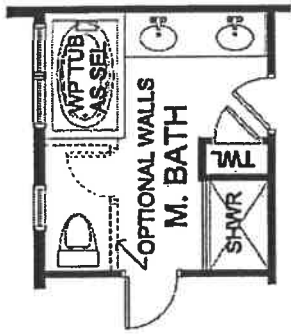
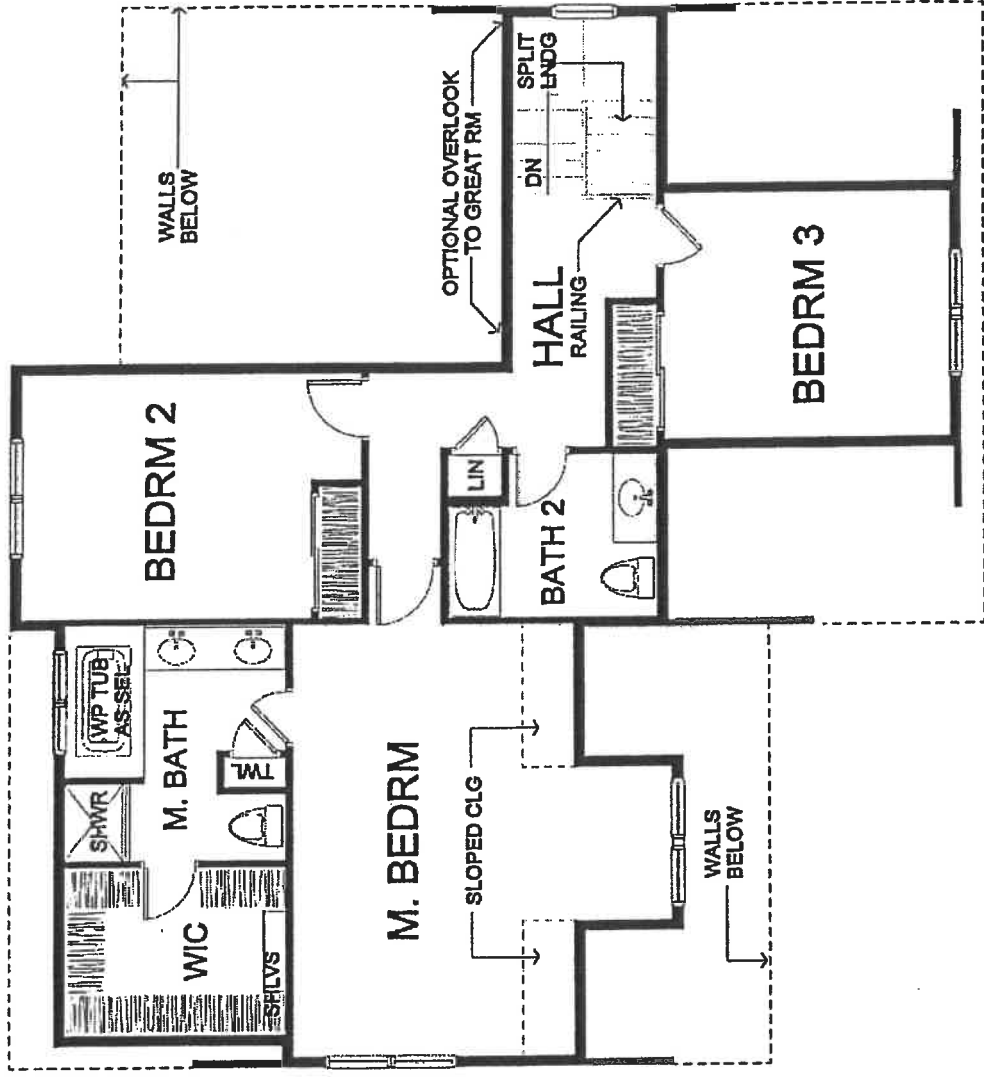
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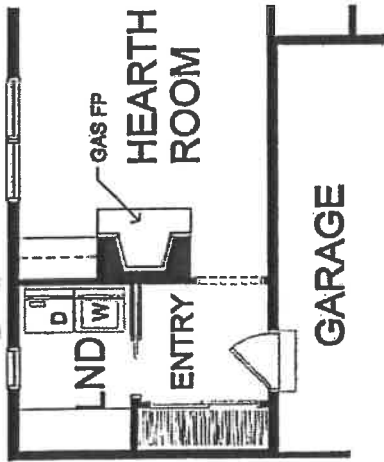
**PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.**



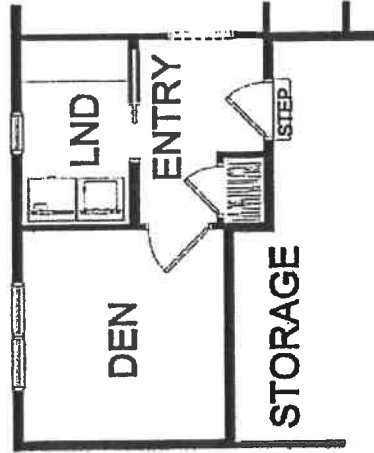


OPTIONAL  
M. BATH

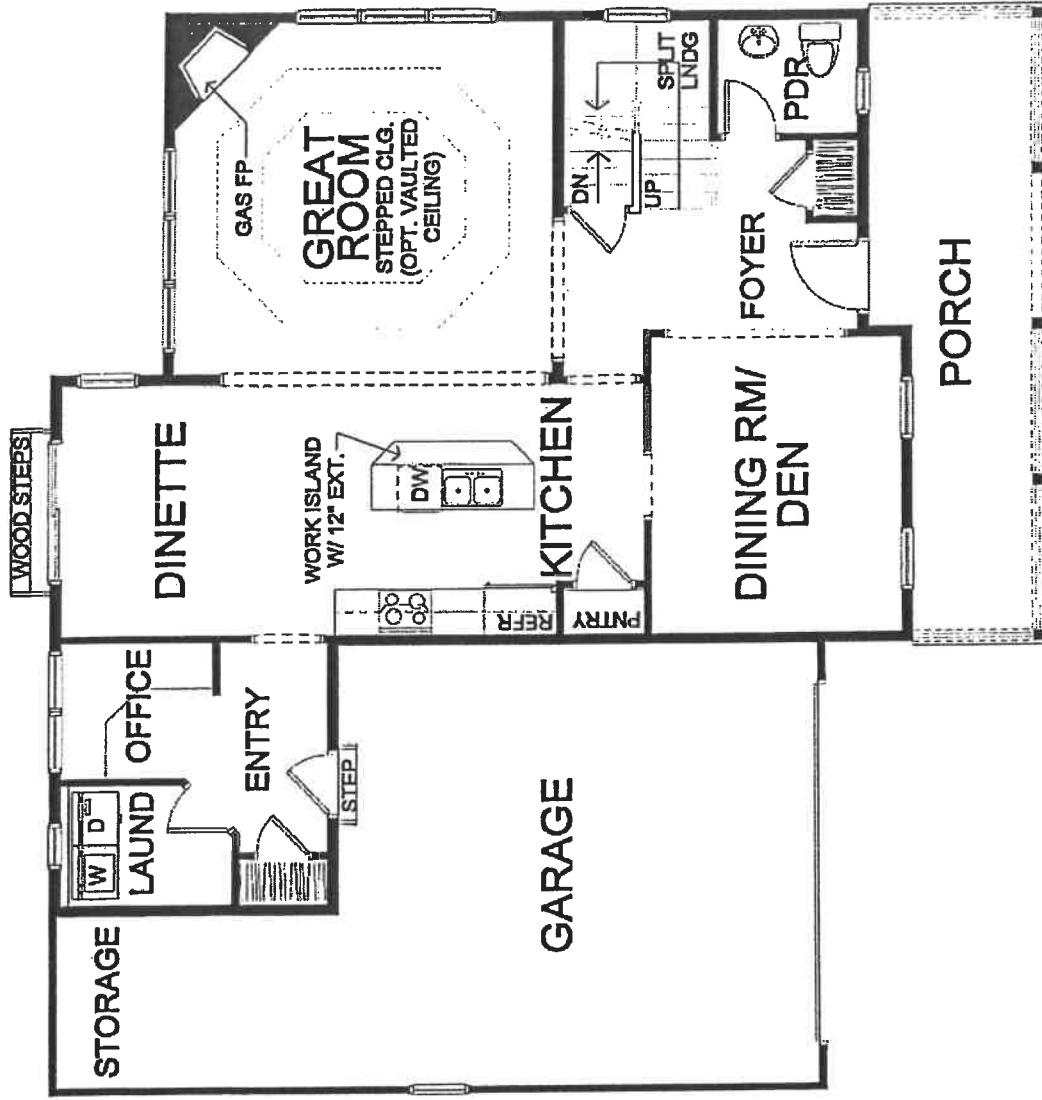
PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.



**HEARTH ROOM OPTION**  
 1246 SQUARE FEET  
 2345 TOTAL SQUARE FEET



**DEN OPTION**  
 1211 SQUARE FEET  
 2310 TOTAL SQUARE FEET



**PATRICK J. MORABITO, A.I.A. ARCHITECT, P.C.**